



# SOUTH POPLAR MASTERPLAN

SUPPLEMENTARY PLANNING DOCUMENT

JULY 2021



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In developing this document the Council have worked closely with the Tfl Spatial Planning team. The Tfl Spatial Planning team support Tfl in delivering the Mayor's strategy and commitment on transport in London and ensure that Tfl is meeting its legal duties with regard to managing transport infrastructure and services by helping planning applicants, the rest of Tfl, the Greater London Authority (GLA), boroughs, and other stakeholders to combine land use and transport planning. The Council have also liaised with the Tfl Property Development team who represent Tfl as landowner of the Poplar DLR Depot and the potential re-development of this site. More information on both these Tfl teams can be found here:

<https://tfl.gov.uk/info-for/urban-planning-and-construction/our-role-in-planning?intcmp=3484>

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Figure 1: View towards Canary Wharf across Poplar (© London Borough of Tower Hamlets 2015)

# 1 INTRODUCTION

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OVERVIEW

AIM AND PURPOSE

ROLE OF THIS SPD



## 1.1 AN OVERVIEW

The South Poplar Masterplan Supplementary Planning Document (SPD) was adopted by the London Borough of Tower Hamlets on 28 July 2021

The SPD supports the delivery of the policies and aspirations contained within the Tower Hamlets Local Plan 2031 (Managing Growth and Sharing Benefits) which was adopted by Full Council on 15 January 2020 and the London Plan which was formally published on 2 March 2021. The SPD aligns with the recommendations contained within the Mayor of London's Isle of Dogs and South Poplar Opportunity Area Planning Framework adopted on 14 October 2019.

This SPD has been prepared in accordance with the National Planning Policy Framework (NPPF) and Town and Country Planning (Local Planning) (England) Regulations 2012. The SPD accords with the new London Plan 2021 and will supplement policies contained within the Tower Hamlets Local Plan 2031 and related site allocations.

## 1.2 AIM AND PURPOSE

South Poplar will see significant development coming forward. At present, the area has a high proportion of social and affordable housing and sustains a vibrant and active community. The aim of this document, alongside LBTH's Local Plan, is to maintain a strong sense of community, while ensuring that the current and future residents of South Poplar benefit from anticipated growth.

Residents of South Poplar should see improved transport capacity and connectivity, the provision of social infrastructure alongside development, the benefits from joined up development across different land ownership and the area protected against the construction impacts of future developments. The masterplan will provide opportunities for social cohesion, optimise land around a transport hub and provide more certainty within the development management process. The masterplan sits alongside LBTH's Local Plan in seeking to bring forward infrastructure in the area to ensure that development does not impact on access to public services for existing communities.

The masterplan will assist in ensuring the vision and objectives set out in the London Plan, the Council's Local Plan and the Mayor of London's Isle of Dogs and South Poplar OAPF are delivered. Through these aforementioned documents, South Poplar has been recognised as a key opportunity and growth area within the borough. This is highlighted through the three allocated sites identified within the masterplan boundary.

Within the South Poplar area, there are several development sites that have come forward, signalling the developer interest in the area. All of the sites within the masterplan are at varying stages of the development process, however they are likely to come forward in the very near future. The masterplan will help ensure these sites are not delivered in isolation and contribute collectively to the wider aims and aspirations of the area. To respond to the various challenges and opportunities presented on each development site, this masterplan seeks to provide high quality, sustainable, deliverable guidance and strategies.

The Council, Greater London Authority (GLA) and

Transport for London (TfL) have identified the need for further guidance in addition to existing adopted policies to assist in ensuring development across South Poplar is equitable, joined-up, and maximises public benefits.

The South Poplar Masterplan has been produced to achieve all these aims. The guidance contained within this document seeks to focus on the design of future developments. The provided guidance, strategies and figures should be used as a tool to inform the design of development proposals at an early stage.

The figures and diagrams within the masterplan document are illustrative and have been interpreted from the London Plan, Isle of Dogs and South Poplar OAPF, Tower Hamlets Local Plan policies and site allocations.

## 1.3 ROLE AND STATUS OF THIS DOCUMENT

The South Poplar Masterplan SPD is a material consideration to help determine planning applications within the designated masterplan area. This means that in addition to satisfying the requirements of national, regional and local planning policies, development proposals will also need to demonstrate how the guidance in this SPD has been considered. The SPD supplements and provides detailed guidance on the policies and recommendations within the London Plan, Tower Hamlets Local Plan and Isle of Dogs and South Poplar OAPF.

The SPD document can be used to provide support for infrastructure funding bids and the development of business cases. The document can also be used by anyone who is interested in understanding the future aspirations for this area set to experience growth and change. Once adopted, its guidance will be applied to the area of South Poplar and Canary Wharf as indicated on the masterplan boundary. It will be relevant to all development applications within this designated area.



Figure 2: Tower Hamlet's Local Plan will guide and manage development across the borough to 2031



Figure 3: Isle of Dogs and South Poplar OAPF provides a framework for growth to 2041.

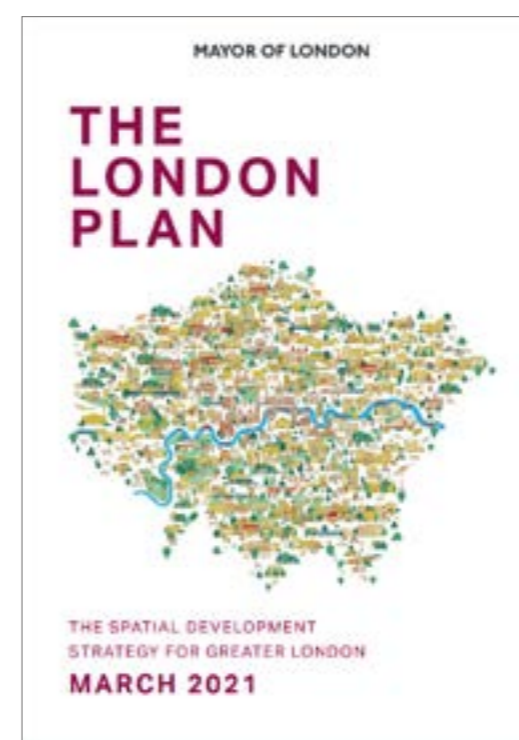


Figure 4: The new London Plan

# 2 CONTEXT

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POLICY POSITION

CONSULTATION AND ENGAGEMENT

MASTERPLAN AREA

HISTORIC CONTEXT

LOCAL CONTEXT

STRATEGIC LOCATION

PERMITTED, PROPOSED AND EMERGING DEVELOPMENTS



## 2.1 PLANNING POLICIES

The South Poplar Masterplan SPD supplements the following policy framework and legislation:

National	<ul style="list-style-type: none"> <li>The Town and Country Planning Act (1990)</li> <li>Planning and Compulsory Purchase Act (2004)</li> <li>Town and Country Planning (2012)</li> <li>National Planning Policy Framework (2012)</li> <li>Planning Practice Guidance</li> </ul>
Regional	<ul style="list-style-type: none"> <li>The London Plan (2021)</li> </ul>
Local	<ul style="list-style-type: none"> <li>Tower Hamlets Local Plan 2031: Managing Growth and Sharing the Benefits (2020)</li> </ul>

### National

The National Planning Policy Framework sets out to ensure the planning system contributes to the achievement of sustainable development. This is pursued with economic, social and environmental objectives. The NPPF also includes guidance for when and why Supplementary Planning Documents should be prepared. The South Poplar Masterplan has been developed in accordance with this guidance in seeking to help facilitate successful applications and deliver the required infrastructure to support housing growth in the masterplan study area.

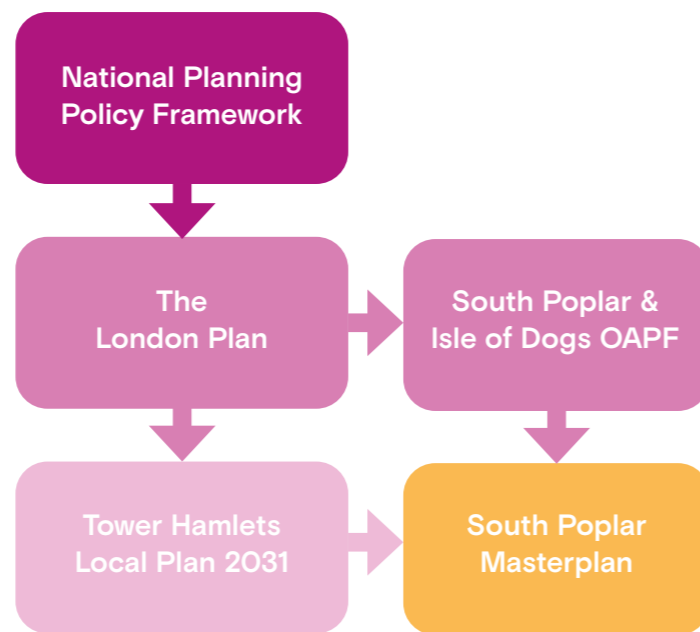


Figure 5: The relationship of the South Poplar Masterplan to existing planning policy

### Regional

The London Plan (2021) identifies the Isle of Dogs and South Poplar as an Opportunity Area. Opportunity Areas are identified as locations with development capacity to accommodate significant new housing, commercial development and infrastructure (of all types), linked to existing or potential improvements in public transport connectivity and capacity. The Isle of Dogs and South Poplar Opportunity Area Planning Framework (OAPF) is a document prepared by the Mayor of London and Transport for London in consultation with Tower Hamlets Council.

The framework was adopted by the Mayor of London on Monday 14th October 2019. The aim of the framework is to provide greater certainty to the community on how development can be influenced and can assist in guiding developers. The coordinated planning document seeks to manage pressures of growth and secure infrastructure delivery. The South Poplar Masterplan SPD has been informed by this framework document and acknowledges the recommendations made.

### Local

The Tower Hamlets Local Plan 2031 (Managing Growth and Sharing the Benefits) was adopted in January 2020. The plan provides relevant borough-wide, area-specific and site-specific guidance for the South Poplar area.

The site allocations, shown in Figure 6, provide a series of land use and infrastructure requirements for development sites.

**Aspen Way** – Located to the north of Aspen Way, this site allocation comprises approximately 6.1 hectares of land and is well located in terms of proximity to transport, town centres and employment areas. The site has multiple landowners, though is largely in Transport for London ownership. The site has capacity to provide a significant contribution to the borough's housing targets, provide employment opportunity for small to medium sized enterprises, support community and social infrastructure and also support the delivery of strategic open space. Local Plan requirements for development include the delivery of open space, improvement/enhancement of the existing pedestrian bridge over Aspen Way, re-provision of the existing DLR depot, New City College campus, community centre and associated football pitches.

**Billingsgate Market** – Located to the south of Aspen way (west of Trafalgar Way), this site allocation comprises approximately 5.74 hectares of land. The site is in public ownership with significant capacity to deliver employment floorspace and new homes to contribute to the borough's targets. Local Plan requirements for development include the delivery of a secondary school (subject to need and demand) and open space.

**North Quay** – Located to the south of Aspen Way (west of Upper Bank Road), this site allocation comprises approximately 3.27 hectares of land. The site is in single ownership, currently vacant and subject to a live planning application. This

site will make a significant contribution to the borough's targets for new employment floorspace and new homes. Local Plan requirements for development include the delivery of open space and the improvement/enhancement of the existing pedestrian bridge over Aspen Way.

All allocated sites have significant opportunity to achieve improved north-south and east-west connectivity.



Figure 6: Site allocation and key policies

- MASTERPLAN SITE AREA
- CANARY WHARF MAJOR CENTRE
- POPLAR HIGH STREET NEIGHBOURHOOD CENTRE
- TOWER HAMLETS ACTIVITY AREA
- TALL BUILDING ZONES
- CANARY WHARF SKYLINE OF STRATEGIC IMPORTANCE
- SECONDARY PREFERRED OFFICE LOCATION
- SITE ALLOCATIONS:
  - ASPEN WAY
  - BILLINGSGATE MARKET
  - NORTH QUAY

## 2.2 CONSULTATION AND ENGAGEMENT

The production of the SPD was informed by input from a range of stakeholders including:

- Local residents
- Landowners and Developers
- Greater London Authority
- Transport for London
- Historic England
- Natural England
- Canal & River Trust
- Local community groups
- Local Tenants & Residents Associations
- Registered providers
- Utilities providers

In line with the requirements of the Town and Country Planning (Local Planning) (England) Regulations 2012 and the Council's Statement of Community Involvement (SCI), a formal public consultation was held between 9 March 2021 and 20 April 2021 where a draft document was published. A series of events were held during the consultation period to further engage with the public. Received representations were considered and, where appropriate, amendments to the draft document were made and incorporated into the adopted SPD. Further information can be read in the Consultation and Engagement Report.

### Landowner Engagement

There are a number of different landowners within the masterplan area. We have worked closely with these landowners to establish site constraints and develop the masterplan approach.

### Early Engagement

Early engagement with the community and key stakeholders for the South Poplar Masterplan began at an early stage. As part of this process, the key aims have been to:

- Involve the public in the engagement process
- Share information about what's happening in the area

- Hear people's aims and aspirations
- Gather thoughts on the emerging masterplan
- Understand key site constraints and opportunities further

The early engagement was undertaken in 4 stages as summarised in the diagram below.



Figure 7: Summary of engagement stages (Image: Soundings)

Opposite is a summary of the early engagement process between September and December 2020.

You will see the following engagement badge throughout the document showing how we have addressed received comments.



### HOW WE ENGAGED

These are the ways in which we communicated with the community.



Email



Digital survey



Online exhibition

### HOW THE COMMUNITY PARTICIPATED

These are the engagement methods and number of participants during the engagement process.



11 stakeholder meetings



835 survey responses



43 online feedback forms



6000 website visits\*



4,500+ digital pieces of feedback

\* digital survey and online exhibition

### WHO WE HEARD FROM

This is a high level summary of who we heard from through the digital survey and online exhibition.



\* digital survey only

Figure 8: Summary of public engagement process (Image: Soundings)



## 2.3 MASTERPLAN AREA

The SPD Masterplan area is informed by the Local Plan Site allocations referenced in section 2.1 (page 4) as well as sites considered integral to the delivery of important spatial goals. In particular, the masterplan area contains the North Quay, Billingsgate Market and Aspen Way site allocations.

The sites where the SPD boundary extends beyond the site allocation areas include an area containing existing housing estates, located south of Poplar High street, the Poplar Business Park and land adjacent to Trafalgar Way. These areas are recognised for their importance in the context of the wider area's development potential and their inclusion is important to fully understand and develop strategies to improve connectivity and the public realm across the South Poplar area.



Figure 9: The masterplan boundary

## 2.4 HISTORIC CONTEXT

'Poplar' was first recorded in 1327 or 1350, the area taking its name from the Black Poplar tree which flourished in wet conditions. The village of Blackwall, to the east has records of shipbuilding from as early as the Middle Ages and Poplar High Street would have acted as a route for passengers travelling to the City. The Isle of Dogs, to the south, originally sparsely populated marshland, remained predominantly farmland with few inhabitants until the establishments of the docks.

### The West India Docks

The surrounding area began to change with the foundation of the East India Dock Company in 1600 and establishment of shipyards at Blackwall. Although it wasn't until the construction of the West India Import Docks in the early 19th century that the area saw rapid transformation into an industrial landscape. The docks formed part of a key trading route to the east from Central London and expanded to become one of the most successful

ports in the world. The southern portion of the Masterplan area historically served the wet docks and the South Poplar neighbourhood to the north grew alongside as a residential community working on and next to the docks.

### Canary Wharf

The advent of containerisation in the mid-twentieth century led to the gradual decline and eventual closure of the docks in 1980. In 1981 the London Docklands Development Corporation was formed with the purpose of regenerating the area. The area subsequently underwent transformation from disused docklands to one of the main global financial centres. Since the 2000s there has been a shift towards more mixed-use neighbourhood with an increase in leisure and residential developments coming forward.

However, despite wholesale redevelopment south of the North Dock to form Canary Wharf, the Masterplan area itself experienced limited development.

### Poplar

The Poplar community grew as a hamlet around a piece of land ceded by the East India Dock Company upon which St. Matthias church was built. The adjacent Poplar High Street became the civic centre of the then Metropolitan Borough of Poplar, housing the town hall and coroner's court as well as shops and inns.

Prosperity of the area fluctuated with the docks and South Poplar in particular was significantly affected by the decline of the shipping industry. The area also suffered considerable bombing in WWII. Post-war redevelopment was predominantly low-rise residential, including numerous council estates, often deviating from the historic grid.

Although Canary Wharf was rapidly becoming an area of concentrated wealth, the simultaneous road expansion programme to improve connectivity to the Isle of Dogs, including Limehouse Link, Aspen Way and Prestons Road Flyover, further increased severance between the areas.

### Recent History of the Masterplan Area

In 1982 the Billingsgate Market relocated from its earlier location in the City to adjacent the north dock, following clearance of the site. It has remained operational since, however there are plans to relocate the market to a new site in Dagenham where it will be consolidated with New Spitalfields and Smithfields Markets.

With the development of Canary Wharf, the disused railway line bisecting the Masterplan area, which closed in 1926, was demolished to make way for the Docklands Light Railway. The DLR opened in 1987 and passenger numbers have since greatly increased as the network expanded. The area immediately north of Aspen Way serves as the DLR operating and maintenance depot.

Most recently, the construction of Crossrail Place in the North Dock and the arrival of the Elizabeth Line improves the area's connectivity to wider London as well as creating new, direct links from Canary Wharf to the Masterplan area.



Figure 10: Historical timeline



# HERITAGE

## Conservation Areas

Part of the Masterplan area falls within the boundary of the St. Matthias Church Conservation Area. The area is characterised by several architecturally significant buildings predominantly dating from the second half of the 19th century. At its heart, nestled in Poplar Recreation Ground, lies the Grade II\* listed St. Matthias Church, the oldest church in Poplar dating from 1654.

The Conservation Area is characterised by a mix of 2 storey terraced houses to the east and west of Poplar Recreation Ground and 3-4 storey public buildings lining Poplar High Street. Of note are mid-Victorian former Town Hall and neo-classical, Portland stone-faced Poplar Technical College.

There are five additional conservation areas, shown on the adjacent map, in close proximity to the site.

1. All Saints Church Poplar Conservation Area: The area surrounding the 1820s Grade-II\* listed church, grounds and rectory, extending south to a small group of early 19th century residential streets and listed buildings
2. Coldharbour Conservation Area: The sole remaining fragment of the old hamlet of Blackwall including the river entrances to the historic dry docks and last examples of the narrow streets which characterised the river front.
3. West India Dock Conservation Area: The area focused around the remaining North Quay warehouses and main dock entrance as the only concentration of West India Dock buildings to have survived the Blitz.
4. Naval Row Conservation Area: A collection of historic buildings, including the listed hydraulic pumping station and former perimeter wall of the docks.
5. Lansbury Conservation Area: Centred on the exemplary Lansbury post-war housing estate.

## Statutory Listed Buildings

There are a number of significant historic assets and designations in and adjacent the masterplan area which are identified on the map opposite.

Of particular significance, the docks and water characterise the southern portion of the Masterplan area and are a reminder of its industrial heritage. The quay walls, copings and buttresses to the docks are Grade I listed.

There are two additional Grade II listed buildings within the Site, Poplar Technical College and Accumulator Tower:



The Poplar Technical College forms part of a cluster of historic buildings along Poplar High Street and contributes to character of St Matthias Church Conservation Area



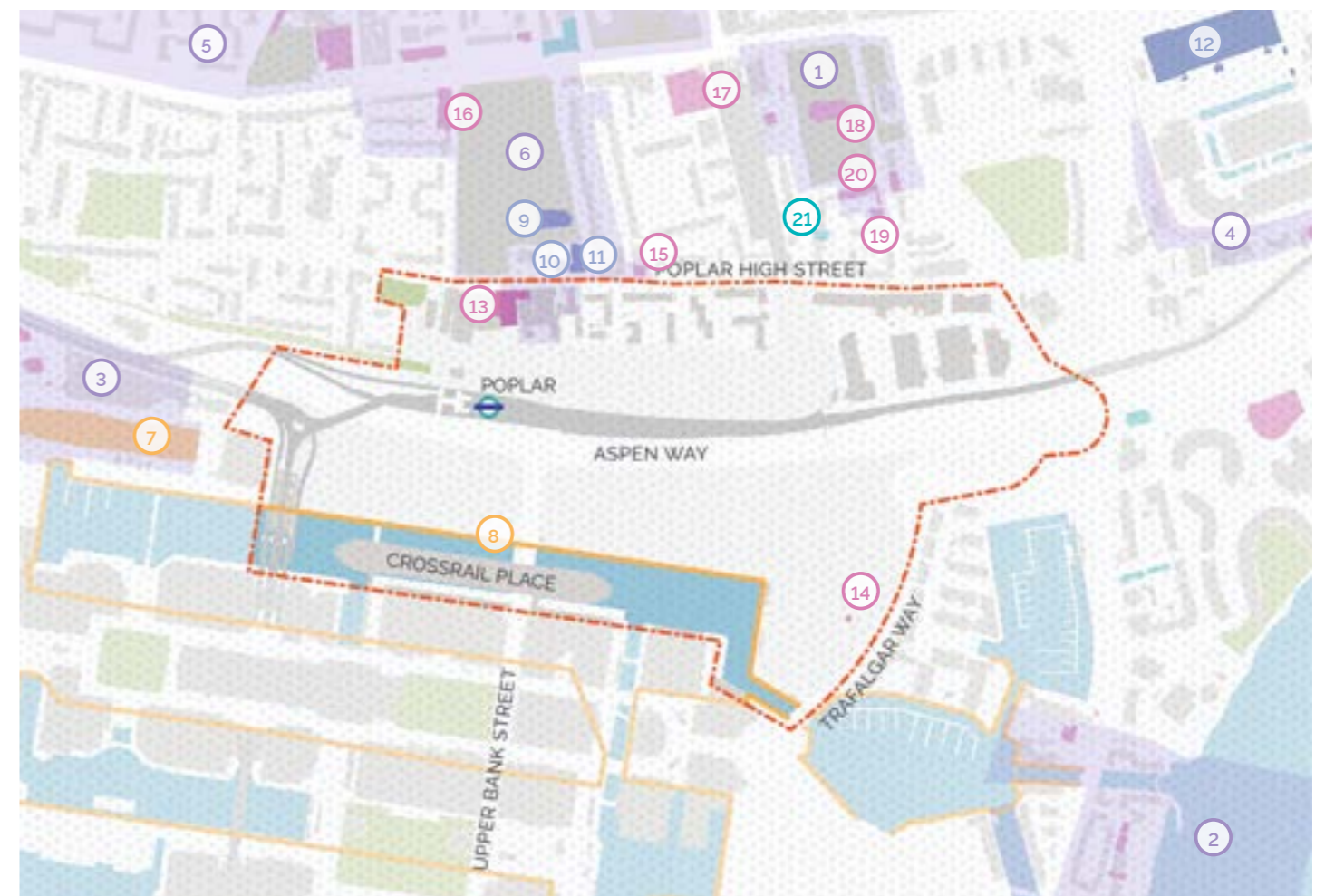
The Accumulator Tower on the west side of Poplar Dock built 1877-8 historically served as a power source for the dockside vanes

## Locally Listed Buildings

There are no locally listed buildings within the Site, however the nearby Hope & Anchor pub recalls when the area was home to numerous inns and pubs.

## Archaeology

The entire masterplan area is a designated Archaeological Priority Area. It includes the remains of social history in the northern part of the area, and extensive dockside heritage and prehistoric archaeology in the low-lying ground to the south. Applicants for new development should engage with the Greater London Archaeological Advisory Service (GLAAS) early in developing proposals.



## 2.5 STRATEGIC LOCATION

The Masterplan area comprises approximately 30 hectares adjacent the Metropolitan centre of Canary Wharf in the London Borough of Tower Hamlets.

To the north South Poplar is an established and vibrant low to medium residential area. In contrast Canary Wharf, which forms the southern boundary of the site, is a high-density business and financial hub of national and international importance.

The Site is well connected via the DLR to the City of London, Stratford and London City Airport. With the arrival of the Elizabeth Line (Crossrail) in 2021 set to increase connectivity, interchange opportunities, transport capacity and provide direct connections to Heathrow airport.

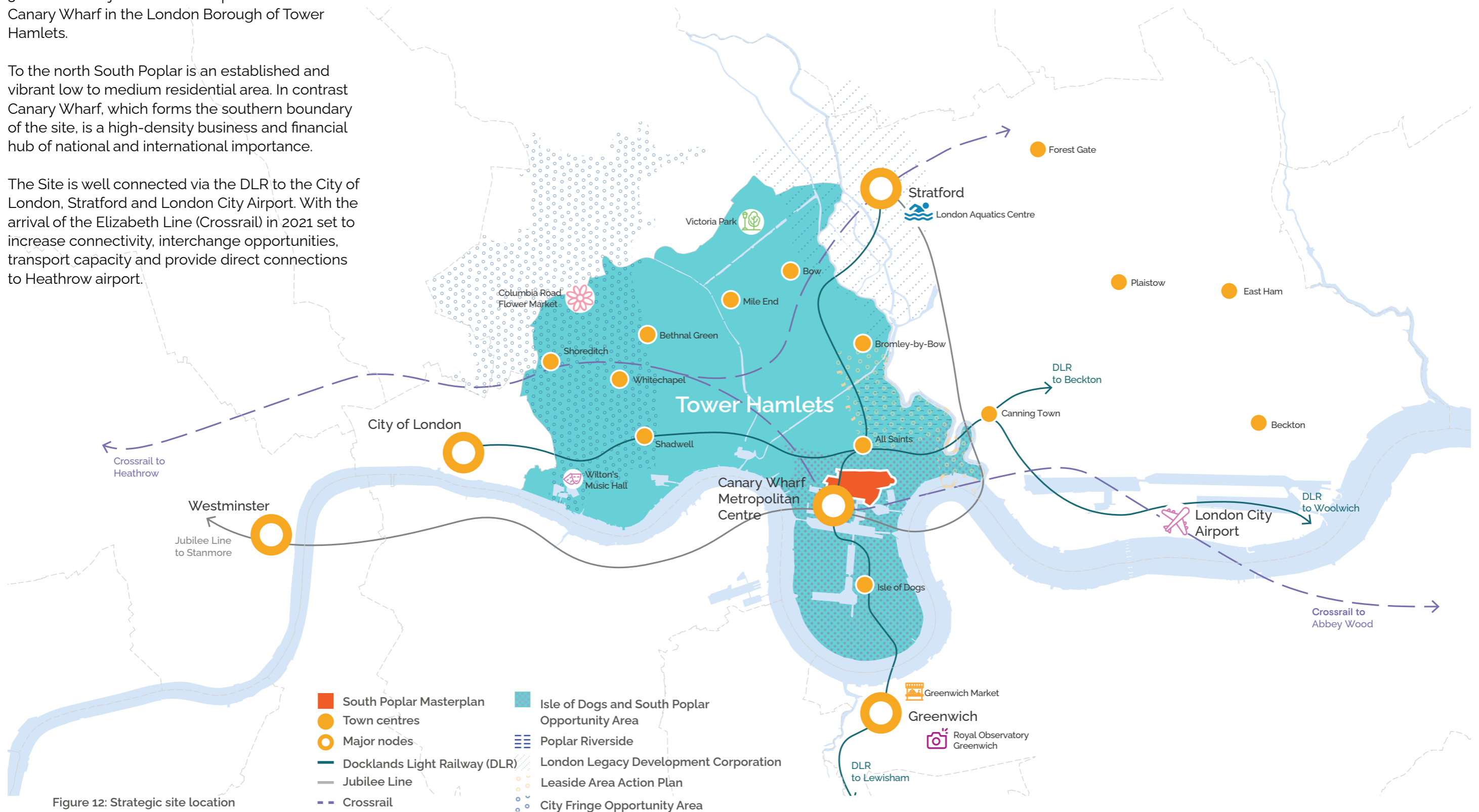


Figure 12: Strategic site location



## 2.6 LOCAL CONTEXT

### Built environment

The masterplan area contains a wide mix of uses and building typologies. South of Aspen Way the dockside is characterised by Billingsgate Market (due to be relocated) and the recently constructed Crossrail Place. Aspen Way and the DLR lines bisect the site east-west, directly north of which is the DLR depot and Poplar Station.

South of North Quay, Canary Wharf is a vibrant, high density centre of international importance whose iconic skyline dominates views across the masterplan area. The northern edge of the masterplan area is of a lower scale and finer grain characterised by post-war social housing, newer developments and a range of supporting uses most notably New City College.

### Public transport

The masterplan area is well connected to public transport. The Poplar DLR station is within the masterplan area and several bus routes run through and border the area. Most of the masterplan area has a PTAL rating between 4 and 5 which will increase with the arrival of the Elizabeth Line. In addition, there are planned improvements to the Jubilee line, DLR and river crossings. However, it is anticipated that further investments above existing plans will be required to meet future demands.

### Public open space

The masterplan area itself has very limited open space and falls partly into an Area of Deficiency of Access to Nature. However, Canary Wharf contains several public, green open spaces and to the north Poplar Recreation Ground is a well-loved community park with sports facilities. The water space and historic docks are considerable public and ecological assets however, access is discontinuous and narrow in sections.

### Characteristics and Social Profile

The masterplan boundary area is made up of two Tower Hamlets' Wards - Poplar and Canary Wharf.

The Poplar Ward is located on the northern side of the masterplan area and contains a population of 6,957 people, accounting for 2.7% of the borough's population<sup>1</sup>. The area of South Poplar itself has a predominance of low density public housing estates with relatively little privately owned housing. Poplar High Street acts as the focal point for this area with a series of shops and services including independent and family run businesses which cater to the needs of the immediate community. Within the Isle of Dogs and South Poplar Opportunity Area, Poplar represents the most deprived ward. This area contains particular concentrations of deprivation in regard to skills, unemployment and overcrowded homes.

The Canary Wharf Ward is located on the southern side of the masterplan area and contains a population of 12,500 people accounting for 4.9% of the borough population. The residential profile of Canary Wharf itself is housed in predominantly high-density privately owned buildings. Canary Wharf is a major international business district and has been designated as a 'Metropolitan Centre' within the newly adopted London Plan. This designation is in recognition of its considerable employment, service, leisure, retail functions and high levels of accessibility. The Area is predominantly characterised by large floor-plate office/commercial buildings, and is currently experiencing an increase in the number of leisure and residential-led uses. The Canary Wharf Ward is one of the least deprived areas in the borough with particular indicators in relation to employment, a high level of skills and few instances of overcrowded homes.

The supplementary planning document seeks to help shape and guide a place that will cater to and respond to the needs of both areas, enabling social integration and opportunities for all who reside, work and visit the masterplan area.

<sup>1</sup> data and statistics sourced from Tower Hamlets Ward profiles 2014

### Economy and Employment

The economic profile of South Poplar is largely focused toward Poplar High Street, which is designated as a Neighbourhood Centre within the Tower Hamlets Local Plan. In addition to retail and convenience stores, Poplar High Street contains a series of community and education led uses including a community centre, a further education college and faith services. Within the South Poplar area, local employment and opportunities for small to medium sized enterprises are limited given the absence of appropriate, affordable workspaces.

The employment opportunities within Canary Wharf are predominantly geared towards the financial and professional services. There is also a diversity of retail and leisure which was initially aimed at the working day population, however has seen change with an increasing amount of residential homes being brought forward.

A range of new services, functions and activities within the masterplan area are set out to help ensure the needs of the local community are met.



Figure 13: Aerial view of the masterplan area



## 2.7 PERMITTED AND PROPOSED DEVELOPMENTS

Several sites within the Masterplan area and immediate context are at varying stages of the planning process and are coming forward for development. The map opposite shows extant relevant planning permissions, current planning applications as well as sites under construction.

The tables below and opposite summarise ongoing development and planning permissions and applications. A summary of emerging development opportunities is provided on page 12, overleaf.



### Developments under construction

- ① **Poplar Business Park (PA/11/03375)**  
The redevelopment of the Poplar Business Park to provide a residential-led, mixed-use development between 3 and 22 storeys providing 392 residential units and 8,104m<sup>2</sup> of light industrial business space. Phase 1 of the development is complete.



- ② **Wood Wharf Masterplan PA/13/02966)**  
Outline Planning Permission was granted for a high-density, mixed-use masterplan comprising 30 buildings up to 60 stories. Blocks were designated as flexible employment and residential uses to provide up to 3,610 residential units, at least 165,000m<sup>2</sup> of office floorspace, hotel, retail, and community and leisure space.



- ③ **Blackwall Reach Masterplan (PA/12/00001)**  
Outline planning permission for the redevelopment of a large site between Aspen Way and East India Dock Road. The residential-led proposals provide up to 1,575 residential units, 1,710m<sup>2</sup> retail, 900m<sup>2</sup> of office floorspace, community space and a replacement school and faith building).



### Extant planning permissions and current applications

- ④ **North Quay Masterplan (submitted PA/20/01421)**  
A masterplan submitted in 2020 comprising predominantly tall buildings up to 225m AOD. The masterplan proposed a very high-density development that is an extension of Canary Wharf. The indicative scheme shows an employment-led development with supporting residential uses.



- ⑤ **North Quay Masterplan (exant permission PA/03/00379)**  
Extant planning permission for an office-led development. The scheme comprises two towers of 43 storeys and 37 storeys with a 23 storey central link building to provide large floor-plate offices and retail together with associated public realm.



- ⑥ **2 Trafalgar Way (submitted PA/20/01402)**  
A student housing-led, mixed-use proposal, submitted in 2020, in partnership with the University College London. The proposals comprise three towers ranging between 28 and 46 storeys connected by a four storey podium with commercial, retail and private amenity space.



- ⑦ **2 Trafalgar Way (extant permission PA/14/01771)**  
In 2009, planning permission was granted for a residential-led mixed use scheme including two towers of 29 and 35 storeys respectively. Several amendments were made to the consent between 2011 and 2015.





## 2.8 EMERGING DEVELOPMENT OPPORTUNITIES

There are a number of sites included within the Local Plan's site allocations which have not yet been subject to a formal planning application. These sites present future development opportunities to support and deliver the masterplan SPD's vision.



### 8 Billingsgate Market

The Billingsgate Market Site, which is jointly owned with the London Borough of Tower Hamlets, forms part of the City of London Corporation (CoL) Markets Co-location Programme. This is a once-in-a-generation opportunity to work with traders, customers, Londoners and central, regional and local government partners to co-locate and consolidate the Billingsgate Market, Smithfield Market and New Spitalfields Market onto a single site at the former Barking Reach Power Station in Dagenham Dock (London Borough of Barking and Dagenham). The release of the three Existing Sites, including the Billingsgate Site, represents a unique opportunity to deliver a significant quantum of office space and new homes, retail and cultural space across London.

This important infrastructure project for London and the South East is subject to a Private Bill process. Should the Market Co-location Programme Bill be passed in Parliament, and subject to necessary planning permissions for the new market, the move to the new location is

expected to take place in circa 2026/7, enabling the transformation of the Billingsgate Site thereafter. Consequently, the CoL has been in regular liaison with the Local Planning Authority and other landowners within the South Poplar Masterplan area during consultation on this SPD to safeguard the future mixed-use redevelopment of the Billingsgate Site once it is vacant.

### 9 New City College

New City College (formerly Tower Hamlets College) owns the Poplar campus, located on Poplar High Street next to the Poplar DLR station. The campus is the largest provider of post-16 education in the borough with 1,200+ students aged 16-18 and over 2,000 adult students. The great majority of students are Tower Hamlets residents as are many of the c. 400 staff. The campus has been offering technical and vocational education for local residents since the opening of the Naval College in 1906, when maritime skills were being taught. The current education provision includes applied, vocational and technical qualifications as well as significant provision of special needs education and English language teaching for those to whom it is not their first language. The university progression rates and qualification achievement rates of students are consistently above national averages.

The current campus is formed of an amalgam of buildings with the oldest ones dating from the turn of the 20th century and the newest one being from 2006. Parts of the campus are no longer fit-for-purpose for education delivery in the 21st century. The layout of existing buildings prevents efficient space utilisation and renders part-refurbishment ineffective. There is an opportunity to redevelop the campus into a state-of-the-art education facility for the local community, a plan which the college has developed over the past few years. The new campus would front Poplar High Street and would seek to incorporate the existing Grade II listed Naval College within the modern college

facility. It would provide an increasing offer for the local community with sports, events and library spaces available for use and hire. The college's proposal assumes that land would be released near the DLR station for an enabling development which would cover the costs of the development of the new campus. The College seek to ensure that this enabling development would consist of 100% affordable homes to support Tower Hamlets in achieving its housing targets.

### 10 DLR Depot

Transport for London (TfL) own the Poplar DLR depot which has a key role in supporting DLR services across the whole network both now and in the future. The ongoing retention of the DLR depot function is essential but there is also the opportunity to deck over the DLR depot and to develop a residential-led mixed use development on top of this deck which will include new public realm and open space, and the potential to provide a new eastern entrance to the DLR station will be explored. There is also the opportunity to provide a new bridge link over Aspen Way, and TfL has been granted Housing Infrastructure Funding from MHCLG which will support the delivery of this significant piece of infrastructure. This funding must be spent by March 2024 and development must also commence in advance of delivery of the new trains which will need to be parked at the DLR depot from March 2024 onwards, although the whole development would most likely not be completed until the early 2030s.

This site has been included in the LBTH Local Plan site allocation of Aspen Way, along with land owned by New City College and the East End Community Foundation. New City College, the East End Community Foundation and TfL Commercial Development jointly prepared a Landowner Area Vision which covered the three sites, in order to shape a comprehensive and collaborative approach to delivery. This landowner-driven vision document, subject to its consistency with the South Poplar Masterplan SPD and Development Plan policy framework,

will help inform the next iterations of design. TfL will continue to liaise with local stakeholders to meaningfully progress the potential development of the Poplar DLR depot site.

### 11 The Workhouse (East End Community Foundation)

The Workhouse site is owned by the East End Community Foundation (EECF), a local charity and grant maker working to alleviate poverty in the East End of London. The Workhouse site, located towards the north-western boundary of the South Poplar Masterplan area, currently provides essential leisure and community facilities to serve existing South Poplar residents and beyond. These facilities are widely used and include football pitches, a multi-use sport hall and changing facilities.

The site is included in the LBTH Local Plan site allocation of Aspen Way and there is an opportunity to redevelop the land for increased community benefit and to achieve the Masterplan's Vision. EECF is committed to providing enhanced community sports facilities, new affordable homes for local residents and working collaboratively with neighbouring landowners to improve the public realm and access routes to Poplar DLR station and Canary Wharf.

# 3 VISION AND PRINCIPLES

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VISION

OPPORTUNITIES

MASTERPLAN PRINCIPLES



### 3.1 A VISION FOR SOUTH POPLAR

By 2031, South Poplar will have a cohesive mix of housing, employment and community spaces within distinctive, inclusive and vibrant neighbourhoods, which have a strong sense of place.

Residents will be able to move around South Poplar, Canary Wharf, Blackwall and the Isle of Dogs easily, with improved transport connections, walking and cycling routes and bridges over Aspen Way. South Poplar will have a thriving local high street and new, affordable workspaces to support local residents, a range of flexible start-ups and small to medium enterprises and new employment opportunities. Residents will also have easier access to employment opportunities in Canary Wharf. Canary Wharf will see new development that will reinforce and strengthen its role as a global business centre. South Poplar will remain a distinct place with a high proportion of genuinely affordable homes, rather than becoming an extension of Canary Wharf.

There will be a sense of belonging and community in the area, created through public spaces and community facilities and employment opportunities accessible to residents from different backgrounds. Aspen Way will be less of a physical barrier, meaning there'll be greater opportunity for communities to come together, forming strong, positive relationships across the area.

Development will be of exemplary quality and capable of accommodating densities to support sustainable places and enhance the positive characteristics of the area. This will result in a greener and more attractive living and working environment, whilst meeting housing needs.

New housing within the area will be delivered with high quantities of affordable housing to help address the needs of people and families on the Tower Hamlets waiting list, including families living in overcrowded accommodation.

The opening of the Elizabeth line will provide increased public transport accessibility and capacity to support the delivery of new homes and jobs to serve both existing residents in South Poplar and future communities.

A significant investment will be made to increase in the capacity of the DLR service along with new trains allowing people to move in and out of the area easily. New and improved links across Aspen Way will enhance walking and cycling across South Poplar. Residents in South Poplar will benefit from new development with improvements in air quality and access to large new areas of green public open space. South Poplar's unique positive qualities will be enhanced and the opportunities within this area will be utilised to create liveable and sustainable neighbourhoods.



### 3.2 KEY MASTERPLAN OPPORTUNITIES

1. Create a buffer or screening to Aspen Way in the short term, whilst realising the potential of Aspen Way itself as a development opportunity in the long term
2. Positively interface with lower density, residential context
3. Connect into and complete the green grid
4. Connect and enhance the cycling super highway along Poplar High Street
5. Contribute to borough designated views and enhance Canary Wharf Skyline of Strategic Importance
6. Connect to and enhance Poplar High Street and the Poplar Neighbourhood Centre
7. Positively interface with St. Matthias Conservation Area and historic assets
8. Strengthen connections and explore potential opportunities around the Poplar DLR station and Canary Wharf Elizabeth Line Station
9. Address severance caused by Trafalgar Way
10. Enhance connectivities and overcome barriers to movement
11. Maximise accessibility to water's edge
12. Protect and enhance historic assets
13. Provide transition in scale from Canary Wharf
14. Form connections and relationships with surrounding opportunity areas including the City Fringe, Lower Lea Valley, Poplar Riverside and the Royal Docks

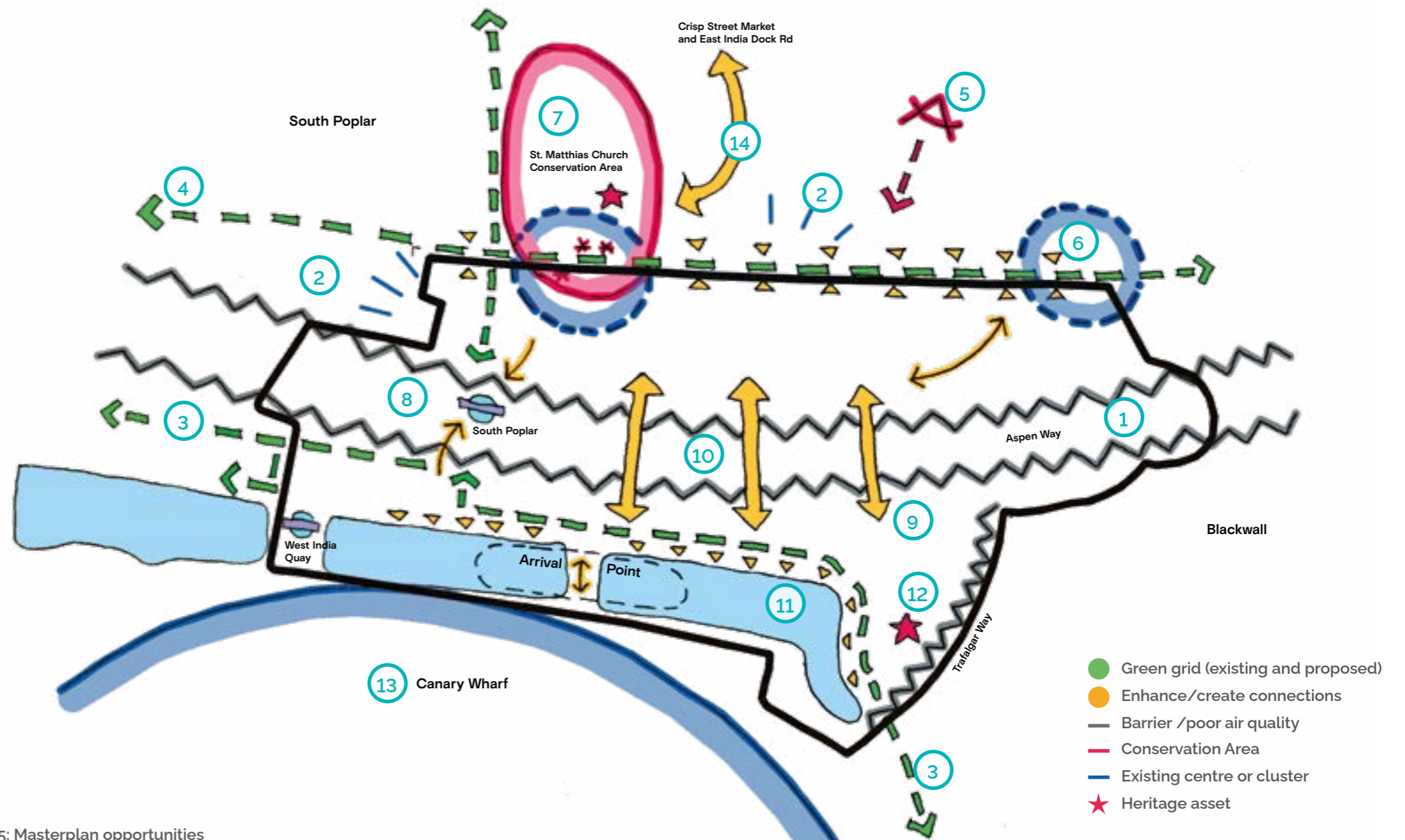


Figure 15: Masterplan opportunities



### 3.3 MASTERPLAN PRINCIPLES

The South Poplar Masterplan is underpinned by the following principles which formed the basis for the first stage of public consultation and informed the design and masterplan guidance in Section 4.



#### GREEN AND OPEN SPACE

The masterplan site has the unique opportunity to connect existing green spaces as well as provide significant new green and open spaces.



#### HERITAGE AND CULTURE

The South Poplar and Canary Wharf area has a rich maritime history stemming from its strategic location as a key trading route to the east from Central London. The South Poplar masterplan aims celebrate this heritage, ensuring that the development enhances and compliments existing assets.



#### LIVEABILITY

Creating healthy, 'liveable' neighbourhoods and cities is increasingly becoming a global priority. Many factors contribute to the urban quality of life including stability, healthcare, culture and environment, education, and infrastructure. While some of this falls outside the masterplan's scope, the form and content of the built environment is a major component.



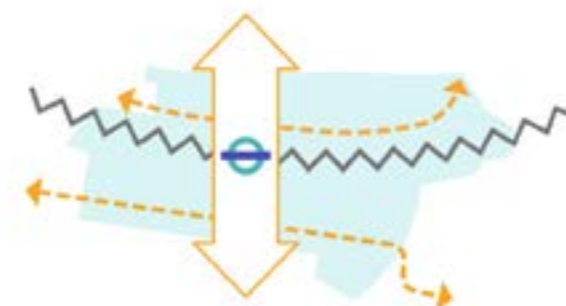
#### USES AND MIX

Tower Hamlets' vision for the wider area is to create a 'cohesive mix of housing, employment and leisure uses within distinctive, inclusive and vibrant neighbourhoods, which have a strong sense of place'. A mix of housing tenures is important to the identity and inclusivity of the South Poplar neighbourhood and will be retained. Growth in the wider area will deliver increased amounts of affordable housing, enhanced community facilities and additional employment opportunities.



#### CHARACTER AND IDENTITY

The South Poplar masterplan will recognise the area's existing assets and provide a strong vision of what the area can look like in the future.



#### CONNECTIVITY

Connectivity is one of the key themes within the masterplan as it aims to enhance existing routes and connections throughout the area, as well as improve the travel experience for cyclists and pedestrians.

# 4 DESIGN AND MASTERPLAN GUIDANCE

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SP1 - CHARACTER AND IDENTITY

SP2 - MOVEMENT AND CONNECTIONS

SP3 - PUBLIC REALM AND OPEN SPACE

SP4 - MASSING AND HEIGHTS

SP5 - USE STRATEGY AND SOCIAL INFRASTRUCTURE

SP6 - LIVEABILITY/SUSTAINABILITY



## 4.1 CHARACTER AND IDENTITY – SP1

The following local plan policies informed SP1 of the South Poplar Masterplan:

### Local Plan Policy Links

- Policy S.SG1 Areas of growth and opportunity within Tower Hamlets
- Policy S.SG2 Delivering sustainable growth in Tower Hamlets
- Policy S.DH1 Delivering high quality design
- Policy D.DH2 Attractive streets, spaces and public realm
- Policy S.DH3 Heritage and the historic environment
- Policy S.EMP1 Creating investment and jobs
- Policy S.TC1 Supporting the network and hierarchy of centres

### SP1

In accordance with Tower Hamlets Local Plan Policies, development should deliver high quality places which respect and positively respond to their context, townscape, landscape and public realm, including the character, appearance and distinctiveness of their settings. To achieve this, the following supplementary guidance for development within the South Poplar Masterplan in relation to Character and Identity is provided:

1. Development should respond to the local character area statements and principles as identified in Figure 16 of the following areas:

- |                    |                                     |
|--------------------|-------------------------------------|
| 1. St Matthias     | 6. Aspen Way                        |
| 2. All Saints      | 7. Canary Wharf Metropolitan Centre |
| 3. Poplar Centre   | 8. North Quay                       |
| 4. Blackwall Edge  | 9. Dockside                         |
| 5. Station Gateway |                                     |

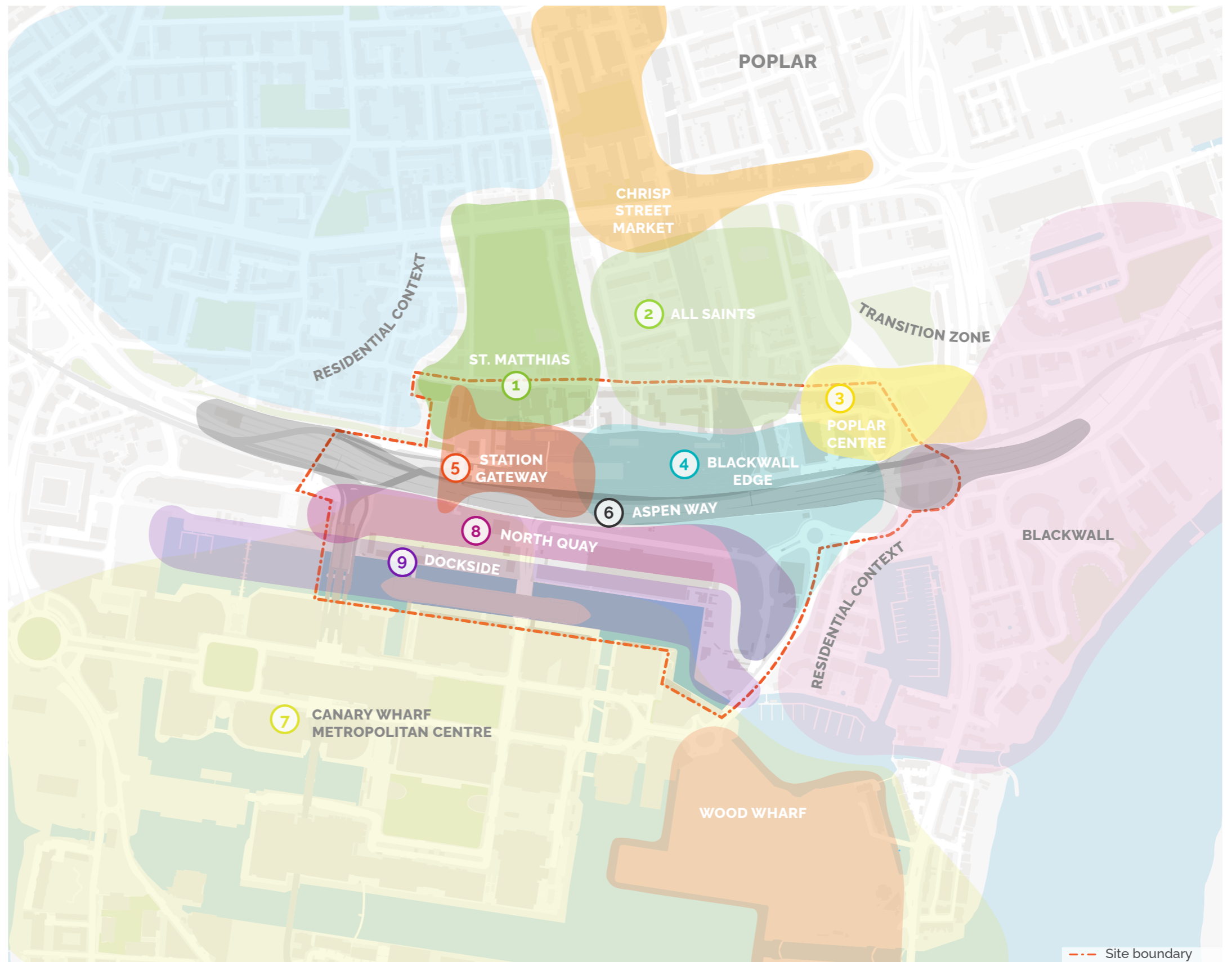


Figure 16: Existing and proposed character areas

## CHARACTER AREA GUIDANCE

The South Poplar Masterplan includes the diverse areas of Poplar and Canary Wharf, both of which have distinct identities and operate in unique ways. In the southern part of the masterplan area and extending to its south, Canary Wharf is a metropolitan centre comprising a high-density business and financial hub of national and international significance. Canary Wharf also has an established and growing mix of residential, retail, leisure and community uses. North of Aspen Way, Poplar is an established, vibrant, multicultural low to medium rise residential area. To ensure the masterplan complements and provides a transition between existing and future communities, nine Character Areas have been identified as highlighted in Figure 19.

The identification of the Character Areas begins by looking at what exists within specified areas and how this can be enhanced. By defining local Character Areas, the masterplan aims to focus on what unique assets exist already, to ensure the area continues to grow as a distinctive series of interconnected places and to avoid homogeneous new development.

The following pages describe the key elements of local character for each of the nine identified areas and urban design guidance. It should be noted that the boundaries of each Character Area are not strictly fixed as often there will be streets and area sections that fall within a transition zone. Applicants are expected to respond not only to the Character Area within which their site is located, but also those adjoining to ensure a holistic and comprehensive picture of the place is captured.



Figure 19: Character areas

## 1 ST MATTHIAS



View east along Poplar High Street with Poplar Recreation Ground on the left showing the attractive historic buildings within and adjacent the masterplan area

### Urban design guidance

New development within and adjacent to St. Matthias Conservation Area must positively interface with historic buildings and context. There already exists a concentration of community uses around Poplar Recreation Ground which has the potential to become a vibrant community hub.



Figure 18: Improve routes along and to Poplar High Street and reinforce the existing cluster of social uses

### Summary of key points

- Reinforce existing 3-6 storey context
- Build on the cluster of community uses adjacent Poplar Recreation Ground
- Retain and enhance heritage assets along Poplar High Street, including the Grade II Poplar Technical College

## 2 ALL SAINTS



View east along Poplar High Street showing the scale of character typical of post-war development characteristic of the area surrounding All Saints Church

### Urban design guidance

The area surrounding All Saints Church extending south of Poplar High Street is predominantly characterised by low to mid-rise, post-war social housing. Any redevelopment in the area must be sensitive to the existing residential and historic contexts of both the All Saints Church Poplar and the St. Matthias Conservation areas.

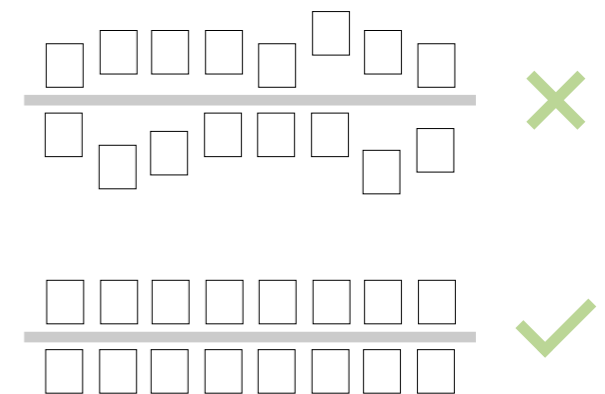


Figure 17: Create a consistent building line and minimise set backs and reinforce the existing urban grain

### Summary of key points

- Establish a consistent building line along Poplar High Street and avoid set backs
- Maintain the finer urban grain and scale
- Improve public realm along Poplar High Street



### 3 POPLAR CENTRE



View west along Poplar High Street showing a linear parade of active uses including conveniences shopping, takeaways and small business

#### Urban design guidance

Poplar Centre currently offers a mix of shops, food and drink premises and local services. It is somewhat hindered by vehicular traffic and poor-quality pedestrian experience particularly at the junction with Cotton Street. New ground floor uses should contribute to active streetscapes and improved pedestrian experience.

#### Summary of key points

- Improve the quality of public realm by incorporating planting, upgrading the CH3 and reducing street clutter
- Improve the pedestrian crossings at Cotton Street junction
- Encourage active, non-residential uses at ground floor

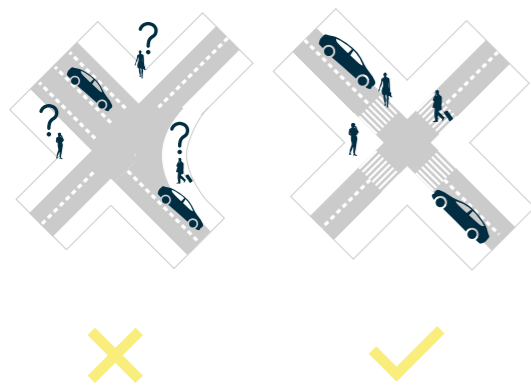


Figure 21: Create clear, prioritised pedestrian and cyclist crossings and avoid multi-staged crossings

### 4 BLACKWALL EDGE



View north along Trafalgar way with the Billingsgate market site on the left and existing medium-density residential context on the right

#### Urban design guidance

The Blackwall area is a predominantly residential area undergoing substantial change and transformation. Its edge should act as an interface between taller and denser development in Canary Wharf and the residential context of Poplar and Blackwall.

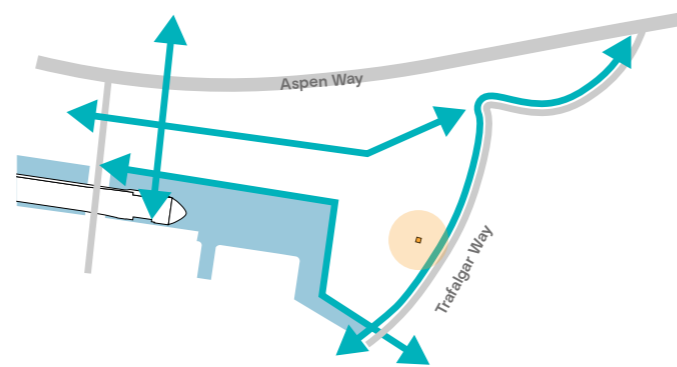


Figure 22: Create new east-west connections and enhance the setting of the dockside and historic Accumulator Tower

#### Summary of key points

- Create new north-south and east-west connections
- Provide a transition in scale between Canary Wharf and Blackwall Basin
- Enhance the setting of the Grade II listed Accumulator Tower
- Improve public realm along Trafalgar Way with a potential new connection to Billingsgate Market site

### 5 STATION GATEWAY



View west along Castor Lane showing the existing Poplar DLR entrance with Aspen Way footbridge above. Access to the station is currently convoluted with poor lighting and wayfinding.

#### Urban design guidance

Improve the existing access to Poplar Station with a hard-landscaped stepped and ramped route incorporating planting and seating. In the future, there is also potential for a secondary eastern station entrance and public square at the landing of the new bridge over Aspen Way.

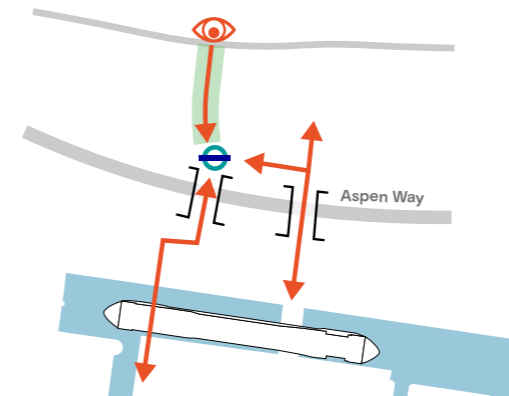


Figure 23: Upgrade existing route to Poplar DLR station and enable a potential eastern station entrance

#### Summary of key points

- Improve access and wayfinding to Poplar Station
- Provide the potential for a future second station entrance with public square
- Upgrade the existing Aspen Way footbridge
- Allow integration with emerging development opportunities at the DLR depot, NCC and EECF sites

### 6 ASPEN WAY



View west along Aspen Way towards existing footbridge. Both the northern and southern edges of Aspen Way lack permeability and are very poor quality pedestrian experiences

#### Urban design guidance

A major highway which supports the economy of London, Aspen Way is also a significant barrier and source of noise and air pollution. Upgraded existing and new bridge connections should create attractive walking and cycling routes with planting and seating. New development facing Aspen Way should act as a buffer to internal streets.

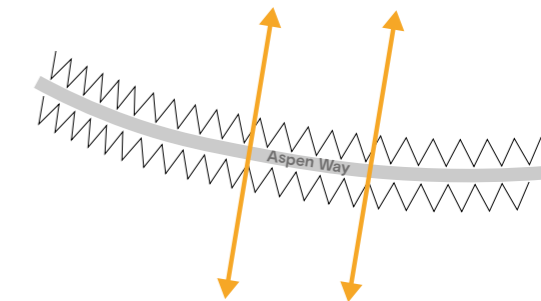


Figure 20: Aspen Way is currently a hostile environment and a significant barrier to north-south movement

#### Summary of key points

- Overcome barriers to movement with new north-south bridge connection(s)
- Mitigate air and noise pollution
- Create a continuous cycle route along the south side of Aspen Way

## 7 CANARY WHARF METROPOLITAN CENTRE



View south across Poplar towards Canary Wharf showing the drastic change in scale, typology and the iconic skyline recognisable throughout London.

### Urban design guidance

Canary Wharf is a key global employment centre with a high density of office space. There has also been an increase in the number of leisure and residential-led developments. While most of this character area is located outside of the South Poplar Masterplan area, it overlaps with the North Quay and Dockside character areas and should be addressed in development proposals.

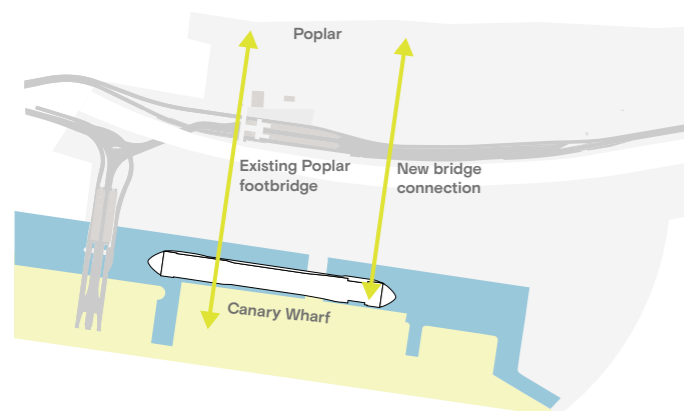


Figure 24: Aspen Way is currently a hostile environment and a significant barrier to north-south movement

### Summary of key points

- Create new north-south connections between the masterplan area, through Crossrail Place, to Canary Wharf
- Uses within and adjacent Canary Wharf should support its re-designation as a Metropolitan Centre
- Development should step down from One Canada Square

## 8 NORTH QUAY



Aerial view across North Quay. In recent years the site has accommodated a range of temporary uses, mostly recently as part of the construction site for Crossrail Place.

### Urban design guidance

The southern portion of the masterplan area should contribute to strengthening Canary Wharf as a global business and major town centre while making it a more vibrant, attractive destination. Although office space will continue to be a major part of the character, it is recognised that there are opportunities for housing, as referenced within the Local Plan site allocations for North Quay and the Billingsgate Market. This character area can also accommodate a mix of supporting uses including food and beverage, retail and community so the area becomes an active place in the daytime and evening. The planned opening of the Elizabeth Line at the new Canary Wharf Crossrail station provides significant opportunity for connectivity across the wider area.

This character area falls within the Canary Wharf tall building zone, in which tall buildings are considered appropriate. New tall buildings must be planned and designed in accordance with Local Plan Policy D.DH6 and policy SP4 of this document.

### Summary of key points

- Buildings should step down in height from One Canada Square
- Provide a mix of residential, employment and community uses that contribute to Canary Wharf Major Centre and Tower Hamlet's Activity Area
- Use development to create a buffer between internal streets and Aspen Way

## 9 DOCKSIDE



View east along the existing generous promenade adjacent the Grade I listed West India Quay, one of the few remaining dockside warehouses. Cafes and restaurants animate the waterfront.

### Urban design guidance

North Dock is one of the masterplan site's greatest assets. The new hard-landscaped dockside promenade should reflect the area's industrial heritage and provide places to sit and enjoy views to the water. Ground floor uses should animate and activate the space for example with cafes, restaurants, and retail.

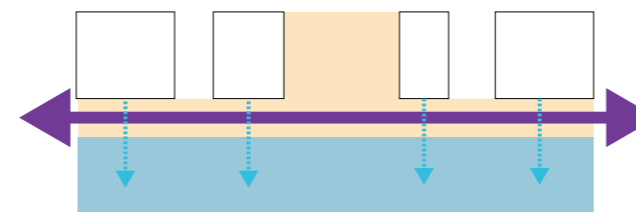


Figure 25: Create a continuous dockside promenade animated by restaurants, cafes and active uses

### Summary of key points

- Create a continuous dockside promenade
- Respect the Grade 1 listed quay walls, copings and buttresses to the docks
- Step buildings down adjacent key public spaces to improve microclimate
- Provide active uses and public spaces adjacent North Dock
- Maximise access and views to the water
- Use a palette of natural materials that reflects the area's industrial heritage

### YOU SAID...

We should recognise the value of the existing character of the area and its heritage assets and ensure that new development respects that character (early engagement)



### WHAT WE DID...

We added detailed descriptions of the different character areas within South Poplar and emphasised their historic features.



### DID YOU KNOW

The Local Plan protects conservation areas and the settings of listed buildings from inappropriate development, preserving the historic townscape.



## A NORTH-SOUTH DIVIDE

There is a clear and dramatic shift in character north and south of Aspen Way.



### View north of Aspen Way towards Poplar

North of Aspen Way is predominantly 3-6 storey townscape, largely residential with a mix of some remaining Victorian terraces alongside post-war social housing. Traditional materials of brick and stone prevail and the urban grain still reflects, in places, the historic street patterns despite the area suffering significant bombing during W/WII.



Figure 26: Aerial views of the site and surrounding context (source: Google Maps)

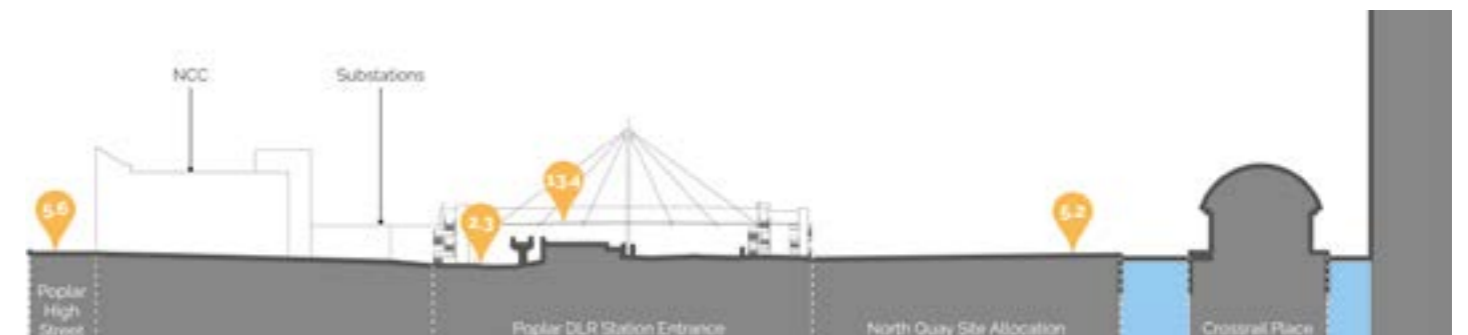


Figure 27: Existing north-south section through the site showing Poplar footbridge and complex level changes

### View south of Aspen Way towards Canary Wharf

South of Aspen Way rises the dramatic and iconic silhouette of Canary Wharf with One Canada Square at the pinnacle. The character is distinctly modern, although there are significant reminders from when the area was one of the world's most successful ports, notably the remaining docks. Large floorplate offices, predominantly point blocks, create a rectilinear street grid of medium-large urban blocks.

## 4.2 MOVEMENT AND CONNECTIONS – SP2

The following local plan policies informed SP2 of the South Poplar Masterplan:

### Local Plan Policy Links

- Policy S.SG1 Areas of growth and opportunity within Tower Hamlets
- Policy S.SG2 Delivering sustainable growth in Tower Hamlets
- Policy D.SG4 Planning and construction of new development
- Policy D.SG5 Developer contributions
- Policy S.TC1 Supporting the network and hierarchy of centres
- Policy D.OWS3 Open space and green grid network
- Policy D.ES2 Air quality
- Policy D.ES7 A zero carbon borough
- Policy S.TR1 Sustainable travel
- Policy D.TR2 Impacts on the transport network

### SP2

In accordance with Tower Hamlets Local Plan Policies, development should deliver a legible and well-defined network of connection routes. This network should integrate incrementally and seamlessly with existing routes. Pedestrian, cyclist and public transport access will be prioritised. North-south barriers to movement should be addressed to increase opportunities for social interaction and support access between Canary Wharf and South Poplar. East-west barriers to movement should be addressed to support access across the masterplan area and beyond. To achieve this, the following supplementary guidance for development within the South Poplar Masterplan in relation to Movement and Connections is provided.

#### 1. Development should:

- Deliver a clear urban block pattern to support walking and cycling desire lines and appropriately define public, communal and private spaces having regard to the indicative layout depicted in Figure 29;
- Link and ensure easy access to and from public open spaces areas;
- Reflect the street sections identified and defined in Figure 35;
- Deliver appropriate vehicular and servicing access routes having regard to the indicative layouts depicted in Figure 30, Figure 31, and Figure 32;
- Provide well designed buildings and sufficient setbacks in locations proximate to dock edges to improve the quality, character and continuity of the identified east-west dockside route having regard to the indicative layout depicted in Figure 35.

#### North-South

##### 2. Development should:

- Deliver improvements and refurbishment to the existing Poplar footbridge over Aspen Way including:
  - Overall repairs and refurbishments, lighting and placemaking opportunities in consultation with TfL/DLR, ensuring no impact on DLR operations and maintenance.
- Deliver improved access and passage at the northern end of the Poplar footbridge to Poplar High Street as depicted in Figure 29;
- Deliver improved access and passage at the southern end of the Poplar footbridge to the western entrance to the Canary Wharf Elizabeth Line station as depicted in Figure 29 and Figure 30.
- Deliver a new crossing over Aspen Way, connecting the Billingsgate Market site to the DLR depot and facilitating access and passage through to Poplar High Street, East India Dock Road and beyond;
- Deliver access and passage from the Billingsgate Market site through to an eastern entrance to the Canary Wharf Elizabeth Line station; and
- Explore additional land bridge and/or deck options, over Aspen Way as depicted in Figure 33.

#### East-West

##### 3. Development should:

- Deliver a pedestrian priority access route along a new dockside promenade located within the identified Dockside character area as depicted in Figure 30 and Figure 31;
- Deliver a new cycle priority route to the south of Aspen Way having regard to the indicative layout depicted in Figure 31;
- Explore the potential for a pedestrian and cycling priority access route centrally through the identified North Quay character area as depicted in Figure 30 and Figure 31; and
- Deliver pedestrian and cycling priority access and passage routes north of Aspen Way as depicted in Figure 30 and Figure 31;

### YOU SAID...

Better walking and cycling connections are needed across Aspen Way.



### WHAT WE DID...

We prioritised the delivery of improvements and extensions to the existing Poplar DLR station bridge and the delivery of a new bridge to link the future development of the Billingsgate Site to the northern side of Aspen Way.



## EXISTING CONNECTIONS

The masterplan site area currently has excellent connectivity to the Greater London area. South Poplar is well connected by public transport and the arrival of the Elizabeth Line at Crossrail Place within Canary Wharf will improve this further. The City of London can be reached within 15 minutes via the DLR and within 30 minutes using various cycling routes. From Poplar Station, the DLR provides connections to the emerging cultural hub at Canning Town (5 minutes) Stratford (20 minutes) and there are further interchange and connectivity opportunities with a direct link to London City Airport (11 minutes).

Despite having excellent city-wide and regional connectivity, local walking and cycling routes within the masterplan area are poor and require significant improvement. Aspen Way, Trafalgar Way, North Dock and live DLR rail lines and depot all create physical barriers and restrict movement within the site and to adjoining areas. In addition to these physical barriers, there exists a significant socio-economic divide between the communities of South Poplar and Canary Wharf. The wider Poplar area is one of London's most deprived areas whilst Canary Wharf is one of the most affluent.

Currently both areas are functional and self-contained in their own right with the only fleeting relationship being the contrasting townscape between the two distinct areas. Aspen Way and the DLR line serve as significant physical and social barriers and unless South Poplar and Canary Wharf are connected more meaningfully, there will continue to be socio-economic disparity with limited services, attraction or complimentary value that these areas provide to each other.

Fragmented land ownership, a poor layout of existing residential estates, ineffective wayfinding and signage, a considerable change in levels and an overall lack of public realm area add to the issues and barriers faced in delivering successful connections. The overall existing pedestrian and cycling experience within the masterplan area is poor and disjointed. Within the site, there is currently limited direct public access to the dockside or to the new Elizabeth Line Station at Crossrail Place.

The South Poplar Masterplan can help to address physical and social barriers to connectivity and improve the experience of pedestrian and cyclists. This can allow a greater uptake in active forms of transportation and bring the areas of Poplar and Canary Wharf closer together. There currently exists only one form of pedestrian access across Aspen Way which is via the Poplar DLR footbridge. Consultation undertaken with local residents identified that Canary Wharf is not seen as place that is inviting for local residents of Poplar despite being so close geographically. The delivery of additional connections and, specifically, pedestrian and cycle infrastructure will be crucial to providing greater opportunity for social interaction between all communities and lessening physical and social gaps.

In terms of vehicular routes, there is currently a good level of connectivity, however congestion along both Aspen Way and Poplar High Street results in negative impacts on the surrounding areas. In particular, air quality and the environment around Aspen Way is poor and additional development both locally and elsewhere in London has the potential to increase vehicular traffic if not well designed and managed.

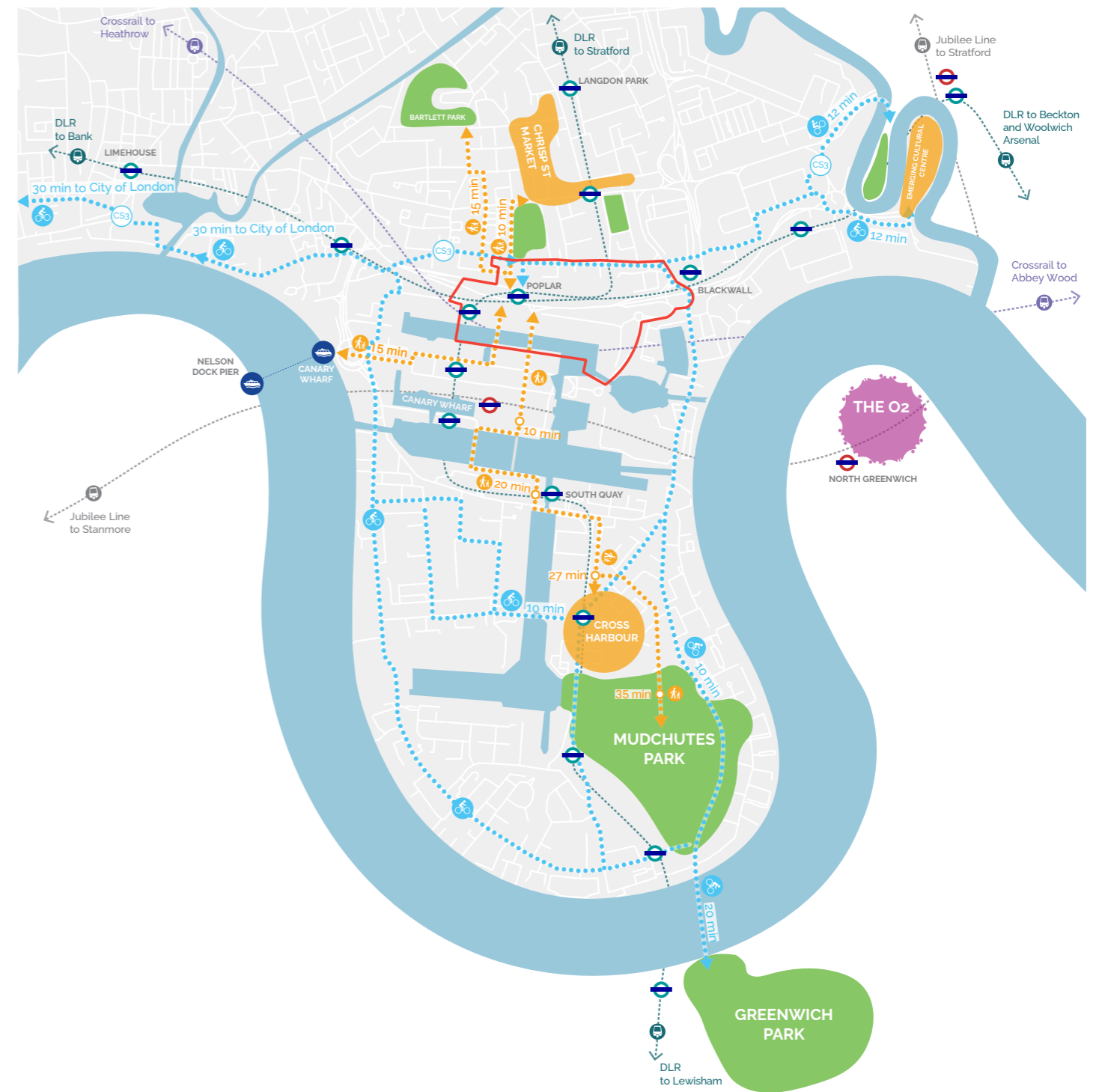


Figure 28: Existing local connections

## CONNECTIVITY STRATEGY

Development should deliver a legible and well-defined network of connection routes to support walking and cycling desire lines and appropriately define public, communal and private spaces as depicted in the diagram opposite.

### YOU SAID...

There should be a continuous walking route along the waterfront.



### WHAT WE DID...

We have safeguarded the dockside edge of the North Quay and Billingsgate sites to ensure a comfortable, walkable and inviting waterfront.

In the online exhibition, 53% of you definitely agreed with our proposed connections, and 40% somewhat agreed.

## PEDESTRIAN AND CYCLING CONNECTIONS

### East-West

**1** In terms of east-west connectivity south of Aspen Way, a new dockside promenade would link West India Quay to Trafalgar Way and the forthcoming development at Wood Wharf. This link would improve wider pedestrian connectivity and help deliver the ambition of a 'Thames-to-Thames' walking route. The dockside provides a unique opportunity to deliver east to west walking and



Figure 29: Principal proposed new and enhanced connections

- MASTERPLAN SITE AREA
- EXISTING ROUTE
- UPGRADED PUBLIC REALM
- PROPOSED KEY CONNECTION
- PROPOSED SECONDARY CONNECTION
- SITE ALLOCATION



cycling routes. Development should step back from the dockside or provide well-designed buildings that ensure no restriction to public access along and to the docks. Development should also explore whether local heritage can be incorporated and reflected.

**2** An east-west cycling route just south of Aspen Way along the motorway, carried through from West India Quay to Trafalgar Way would provide a continuous link from as far as Westferry to Leamouth. This would provide an additional cycling connection through the borough and provide an alternative to the Cycle Super Highway 3 located along Poplar High Street.

**3** A central pedestrian and access route south of Aspen Way would be a key connection, linking the sites of North Quay and Billingsgate Market, then through to Trafalgar Way and beyond.

**4** Additional east-west connections north of Aspen Way could also embed the proposed redeveloped DLR depot site into the wider street network.

#### North-South

**5** The existing Poplar footbridge currently provides north-south access between the Poplar DLR station over Aspen Way to the northern boundary of the North Quay Site. The Poplar DLR service currently provides the predominant public north-south transport link in terms of everyday movement through to West India Quay, Canary Wharf and beyond. An improved pedestrian experience over the Poplar footbridge should be delivered, allowing a high quality, inviting journey to overcome the perception of barriers to movement. This improved bridge should be linked to the north and south via newly created public open space areas and ramps and steps to address the level changes in the area in a manner that is fully accessible (refer to Guidance Point SP3).

Upgrades to the Poplar footbridge should be delivered as a short-term priority to address north-south severance. Approach routes from both

sides should be legible and inviting to encourage more people to walk and cycle between the Isle of Dogs and South Poplar as well as providing a seamless connection for passengers arriving at Poplar station. This bridge has a critical linking function to play in advance of additional future connections between the Poplar DLR depot and the Billingsgate Market site.

With an increasing residential population both sides of Aspen Way, additional bridge and connection options will be important to further support growth over the medium or long term. Providing multiple, safe and attractive pedestrian and cycling priority links over Aspen Way is crucial to ensuring the overall integration and cohesion of the South Poplar and Canary Wharf communities.

Connections to the north of the masterplan study area from Chrisp Street Market and East India Dock Road towards Poplar High Street and through to the Poplar DLR station are poor. An upgraded and additional link between these areas will further improve movement and strengthen walking and cycling connections to the Poplar DLR station and Canary Wharf Elizabeth Line Station.

**6** A second bridge connection over Aspen Way should be located further east of the Poplar footbridge and would connect to an eastern entrance to the Canary Wharf Elizabeth Line Station and the dockside promenade across to Poplar High Street. This would also allow connection into a potential new eastern entrance to the Poplar DLR station.

A new bridge over North Dock, connecting the Billingsgate Site to the Canary Wharf Elizabeth Line Station should be provided to enhance the accessibility of the site and improve connections from Poplar to public transport. This bridge should be designed to ensure that it does not limit navigation width or height for boats using the dock.

Enhanced pedestrian connections to and along Poplar High Street should be investigated to allow

robust links from the proposed second bridge connection over Aspen Way and to allow a legible, defined and safe connection to Poplar High Street and beyond.

The delivery of an improved footbridge crossing and a secondary crossing over Aspen Way is critical to provide access to the public and to bridge the gap between the areas of Poplar and Canary Wharf and bring the two communities together. The Council expects applicants for development sites within the masterplan area to work together with other applicants and landowners of neighbouring sites, to coordinate the approach to the delivery of connections and public realm. This will ensure the delivery of such spaces and links is safeguarded in a context with limited opportunities.

## VEHICLE CONNECTIONS

Whilst active forms of travel across South Poplar will be prioritised, vehicular access to new developments and buildings will still be necessary for servicing, accessibility and emergency services. Two new potential east-west vehicular routes are illustrated on Figure 32 with controlled access. If both routes are brought forward, these would be pedestrian and cyclist priority. North Quay and the Billingsgate Market site could be serviced from the identified central east-west route potentially connecting Hertsmere Road to Upper Bank Street and Trafalgar Way. Two access points to the DLR Depot would be provided from Castor Lane and Harrow Lane. Emergency vehicle and servicing of plots above depot from new east-west routes connecting to Poplar High Street.

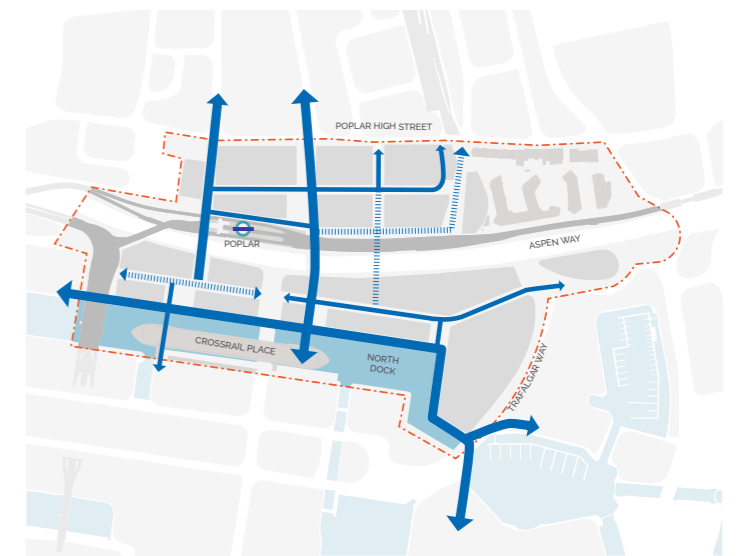


Figure 30: Proposed pedestrian connectivity

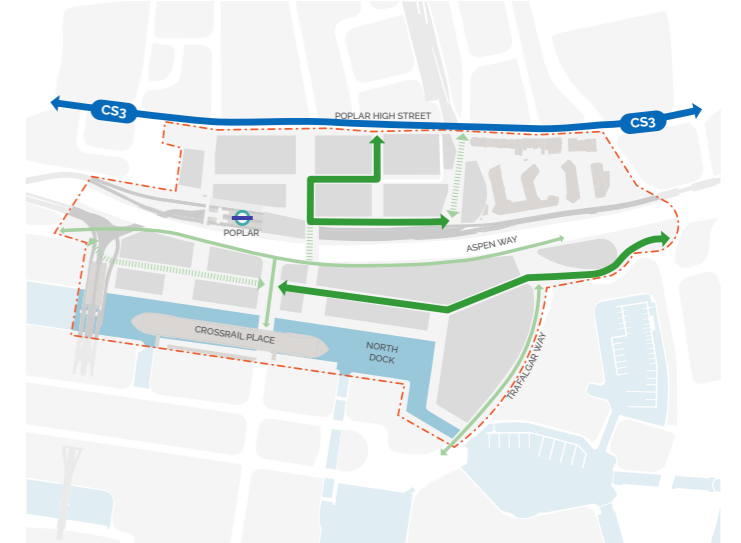


Figure 31: Proposed cycling connectivity

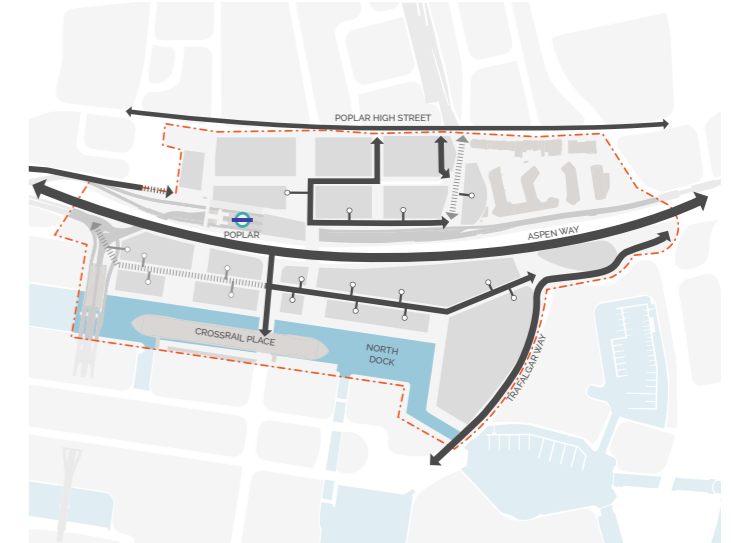


Figure 32: Proposed vehicular connectivity

## ASPEN WAY DECK OVERBUILD

To address the severance caused by Aspen Way, various strategies and projects have looked at the potential for new links between the Poplar DLR Depot and the Billingsgate Market Site.

The proposal to construct multiple land bridges and/or a deck over the DLR tracks and Aspen Way has previously been explored to create north-south link(s) to improve integration between the Isle of Dogs and South Poplar. A deck structure and multiple new bridge links have been aspired to within the Council's Local Plan and supporting evidence base, as well as within the Isle of Dogs and South Poplar OAPF. The aspiration for multiple links and potential decking over Aspen Way would provide significant public benefit to the masterplan area and wider regional context. These initiatives could allow seamless physical and social integration across the motorway, unlock additional capacity for new homes, jobs and open space.



Figure 34: Extract from the Isle of Dogs and South Poplar OAPF showing the objectives for new connections

The delivery of these additional infrastructure items are considered aspirational, but challenging and subject to financially viable options coming forward through development and potential wider public contributions. Whilst landowners and the LPA should collectively explore the potential for the delivery of these infrastructure projects, the aspiration for the deck and additional bridge links should not inhibit alternative development outcomes in being designed and put forward for determination.

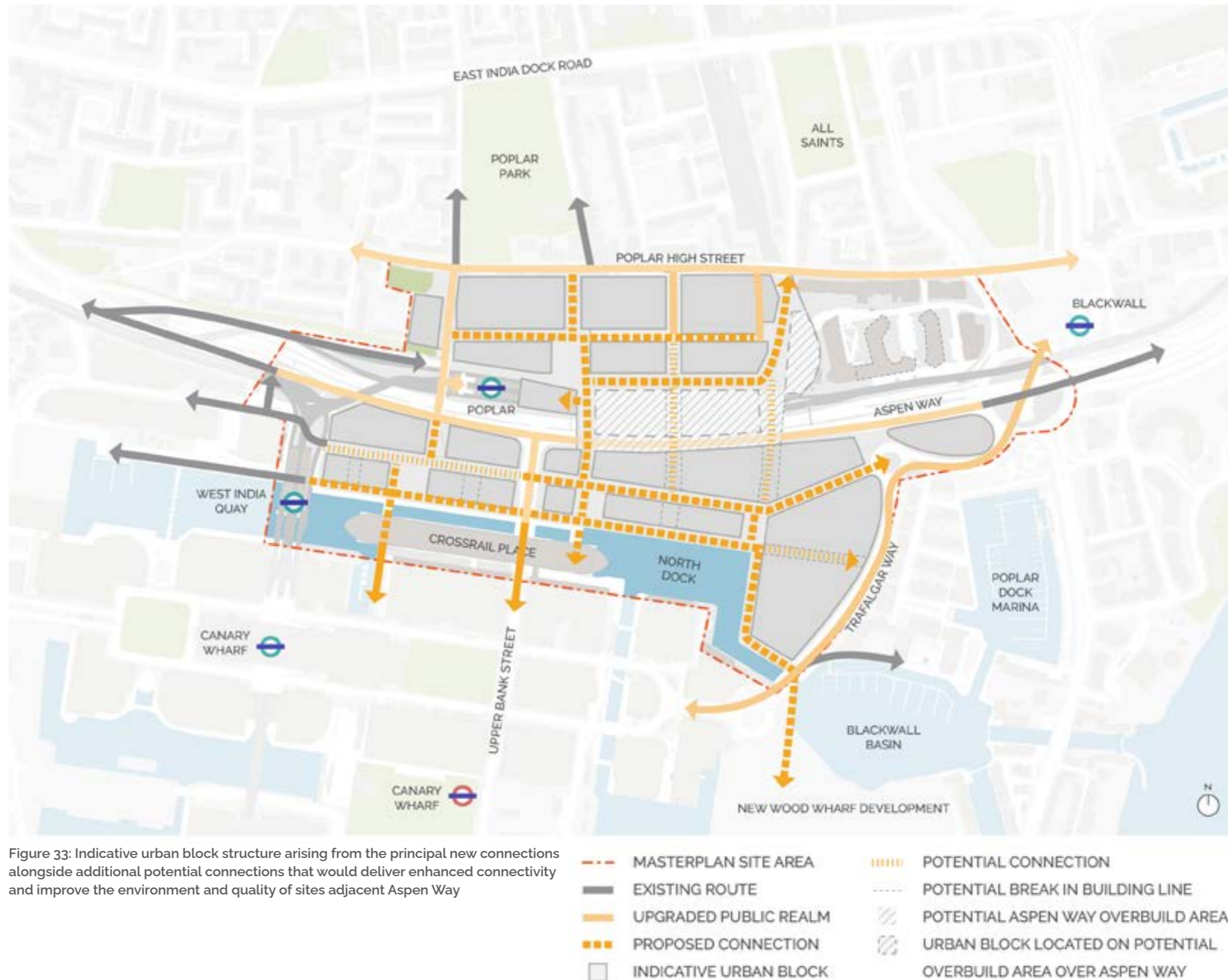


Figure 33: Indicative urban block structure arising from the principal new connections alongside additional potential connections that would deliver enhanced connectivity and improve the environment and quality of sites adjacent Aspen Way



## Viability and Deliverability

The construction of a deck over Aspen Way raises significant challenges given the constraints of the motorway and adjoining sites. Aspen Way is a strategically important, operational motorway that services the wider area. Any closures and/or restrictions to this motorway would have wide-ranging impacts on the highway network. The live and operational DLR lines running adjacent to Aspen Way (as well as level variations) and the DLR depot function present further constraints in the delivery of a deck structure.

The aforementioned potential impacts would be exacerbated if the emerging development opportunities on the adjacent DLR Depot and Billingsgate Market sites come forward prior to the construction of the deck. In such a scenario, access to construct the deck would be further constrained, which may result in increased impacts on Aspen Way as well as the length of time needed to construct the deck.

In terms of financial viability, TfL has been granted £20 million as part of the Central Government's Housing Infrastructure Fund (HIF) aimed at assisting in the unlocking sites with the potential to deliver new homes. This time-restricted funding relates to the delivery of a single bridge link (referred to as item 6 in Figure 29) over Aspen Way as well as the redevelopment of the Poplar DLR depot site.

No funding has been secured to deliver the aspirational Aspen Way Deck or additional bridges. Whilst no funding has currently been secured, possible opportunities for funding sources include through public sector bodies – Central Government schemes, Homes England, GLA, or through local and regional authority schemes. It is acknowledged that unless substantial funding is secured, the delivery of multiple bridge links and/or the Aspen Way deck will not be financially viable.

The phasing of adjacent development sites raises additional concern where the sequencing and

construction methodology of the infrastructure provisions need to synchronise.

Management of Aspen Way is currently operated through a DBFO contract with a private third party and will not return to TfL's full management responsibility until 2031. The ability therefore to carry out works over the motorway would be financially constrained until the DBFO contract has expired.

### Delivery of Masterplan Vision and Aims

The masterplan has been prepared acknowledging the challenges in bringing forward the aspirational, significant infrastructure provisions. It is accepted that the considerable financial cost, technical and operational constraints in delivering the structure may result in it being unachievable to deliver the deck and/or additional bridge links over Aspen Way.

The masterplan's vision and aims would still be achieved without these infrastructure provisions as the area would be supported with the delivery of a single additional bridge link and improved existing footbridge over Aspen Way which are the primary connections proposed as part of this SPD.

The provision of principal proposed new and enhanced connections as depicted in Figure 29 are considered deliverable regardless of the potential additional bridges and deck over Aspen Way.

### Scenario Based Approach

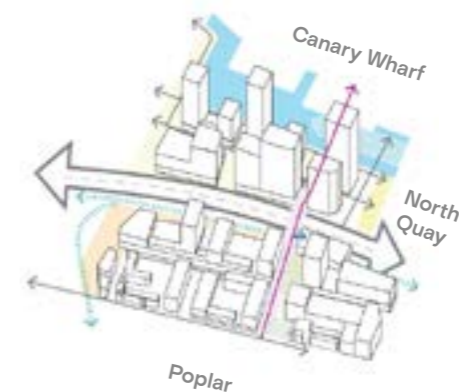
The illustrative figures opposite demonstrate how the development of Aspen Way and adjoining sites could come forward in alternative scenarios.

## Scenario

### Single bridge

The TFL depot and Billingsgate sites come forward independent of any future development over Aspen Way

- + Least coordination and sequencing considerations between sites required
- + Minimal impact on Aspen Way, highway network and DLR/depot operations
- + Single bridge link can be delivered within HIF funding timescales
- Less cohesive public realm
- Noise and air pollution to be mitigated through development of adjoining sites

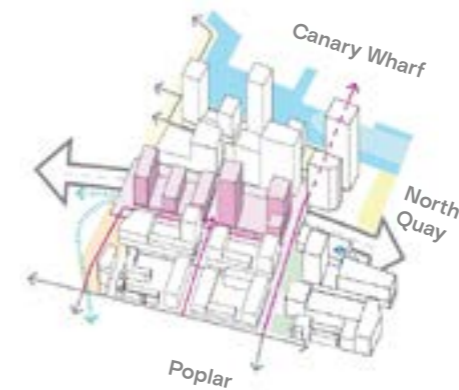


A single bridge from Billingsgate market site to DLR depot overbuild. (Precedent: Stratford Station Bridge)

### Maximum development potential

The TFL depot and Billingsgate sites come forward alongside and in tandem with overbuild development

- + Maximises development potential across the sites
- + Opportunity to create a cohesive public realm
- + Improved views, noise and air quality
- Complex sequencing and coordination required between sites
- Less viable option due to DBFO contract implications, funding challenges, and further technical investigation required
- Physical site constraints and resultant impact on construction

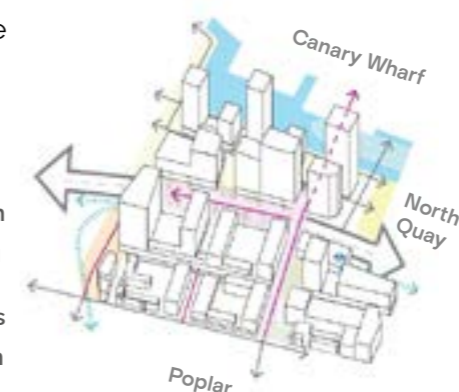


Overbuild can be constructed in sections from either Billingsgate or DLR depot site

### Future development

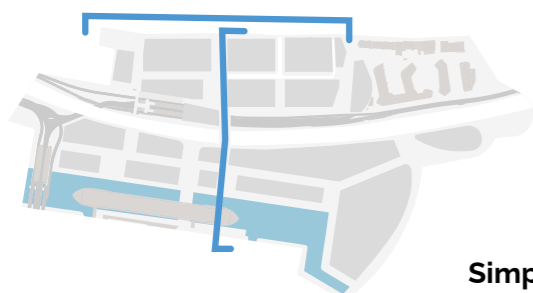
The TFL depot and Billingsgate sites come forward initially and a deck structure is constructed at a later stage

- + Opportunity for a new green link/park at high-level
- + Improved frontages and views to TFL Depot and Billingsgate developments
- + Improved noise, air quality and public realm
- Deck would need coordination with existing developments on Billingsgate and TFL Depot sites, mindful of Aspen Way frontages
- Limited development potential over Aspen Way
- Consideration of single additional bridge link over Aspen Way



Construction of an overbuild after the DLR and Billingsgate sites come forward will likely require closure of Aspen Way

# KEY EAST-WEST CONNECTIONS



**Simpson Road**  
Continuous east-west link created along Simpson Road to unlock and provide access to the TfL Depot site and link new developments



**Linear Park**  
A new linear park above the Depot providing access and amenity for adjacent development



**Potential Aspen Way Overbuild**  
New landscaped pedestrian and cyclist route over Aspen Way that could link to the DLR depot and potential Aspen Way overbuild

**North Quay internal route**  
Pedestrian priority central link through Billingsgate with potential continuation west through North Quay site



**Dockside Promenade**  
Continuous, landscaped east-west connection along the dockside animated and activated by retail, cafes and restaurants

**Poplar High Street**  
Existing key east-west route. Reinforce exiting townscape and upgrade public realm and the Cycle Highway 3 infrastructure

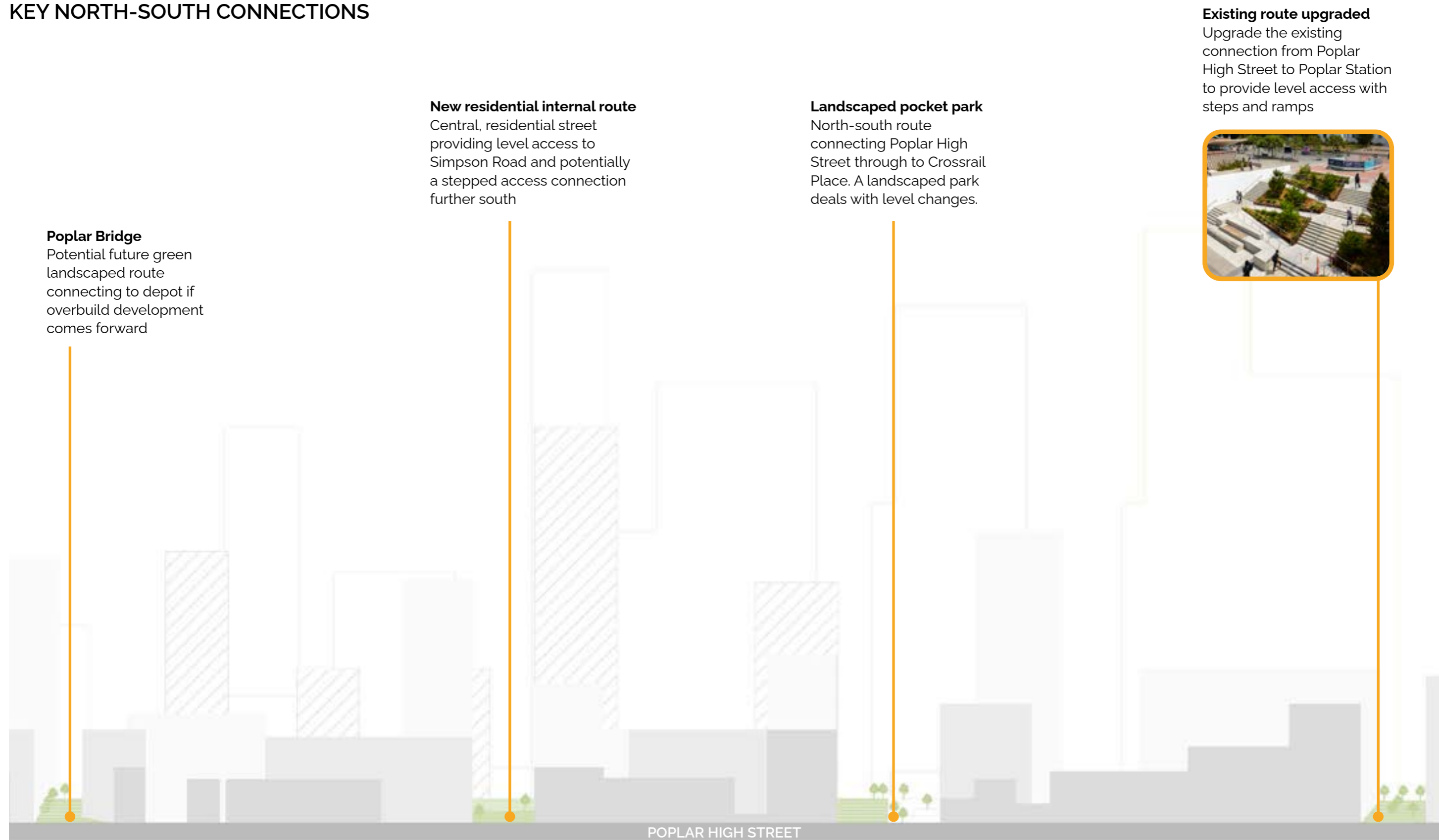


▨ Potential Aspen Way overbuild development site

Figure 35: Key east-west connections



# KEY NORTH-SOUTH CONNECTIONS



**Poplar Bridge**  
Potential future green landscaped route connecting to depot if overbuild development comes forward



**New residential internal route**  
Central, residential street providing level access to Simpson Road and potentially a stepped access connection further south

**Landscaped pocket park**  
North-south route connecting Poplar High Street through to Crossrail Place. A landscaped park deals with level changes.

**Existing route upgraded**  
Upgrade the existing connection from Poplar High Street to Poplar Station to provide level access with steps and ramps



Figure 36: Key north-south connections

 Potential Aspen Way overbuild development site  
 North Quay indicative masterplan submission

## 4.3 PUBLIC REALM AND OPEN SPACE – SP3

The following local plan policies informed SP3 of the South Poplar Masterplan:

### Local Plan Policy Links

- Policy S.SG2 Delivering sustainable growth in Tower Hamlets
- Policy D.SG3 Health impact assessments
- Policy D.SG5 Developer contributions
- Policy S.DH1 Delivering high quality design
- Policy D.DH2 Attractive streets, spaces and public realm
- Policy D.H3 Housing standards and quality
- Policy S.CF1 Supporting community facilities
- Policy S.ES1 Protecting and enhancing our environment
- Policy S.OWS1 Creating a network of open spaces
- Policy S.OWS2 Enhancing the network of water spaces

### SP3

In accordance with Tower Hamlets Local Plan Policies, development should address the physical and social severance caused by Aspen way and existing physical barriers by contributing to a network of accessible, well-connected and sustainable open spaces. Development will need to contribute to community cohesion, creating a positive sense of place and provide a useable, inviting and sociable network of open spaces. Public spaces and areas should be well planted, landscaped with green open space areas enhancing the biodiversity values of the borough, contributing to the objectives identified within the Local Biodiversity Action Plan. To achieve this, the following supplementary guidance for development within the South Poplar Masterplan in relation to Public Realm and Open Space is provided:

1. Development should:
  - a. Deliver and manage high quality public open space areas and routes with regard to the indicative layout depicted in Figure 39;
  - b. Deliver and design public open space areas and routes in coordination with neighbouring sites in order to ensure efficient use of land and avoiding a 'piece-meal' approach;
  - c. Enhance legibility and ensure a clear distinction between public and private spaces;
  - d. Protect and enhance the waterside dock setting, ensuring public accessibility along the entire North Dock waterfront;
  - e. Sufficiently step back from dockside edges to improve the quality, character and continuity of the identified dockside route; and
  - f. Ensure a high quality of spaces around and between buildings and movement through the area.
2. Development should:
  - a. Acknowledge and incorporate local heritage assets;
  - b. Incorporate elements to support health, well-being and education;
  - c. Enable the activation of new and existing spaces through temporary uses, activities, design and public art; and
  - d. Ensure public open spaces areas are accessible from other public areas surrounding the new provision and marked to other open spaces.
3. Development should:
  - a. Ensure public open spaces areas and routes are provided with high quality soft and hard landscaping features;
  - b. Incorporate elements of greenery including those that support and improve biodiversity; and
  - c. Incorporate elements of sustainable urban drainage systems.

### YOU SAID...

The area needs more open and green space.



### WHAT WE DID...

We safeguarded open space within larger development sites and designated their location along key connection and movement routes.



Figure 37: View of Poplar Recreation Ground with St. Matthias Church in the foreground and Canary Wharf in the background



## EXISTING PUBLIC REALM AND OPEN SPACE ACCESS:

To the north of the masterplan area, Poplar Recreation Ground and Bartlett Park are well used, important public spaces enjoyed by residents, visitors, and students. Canary Wharf to the south has several attractive landscaped squares and green spaces including the proximate Cabot Square and Canada Square Park. However, the site area itself currently has very limited open space opportunity and insufficient access to nature.

The water space and historic docks are one of the site's greatest assets, however public access to these areas is discontinuous. Additionally, physical barriers and poor connectivity between South Poplar and Canary Wharf decreases accessibility to the existing nearby quality public spaces. The Aspen Way motorway and adjacent land currently suffers from poor air quality with nitrogen dioxide levels reaching over 60 micrograms per cubic metre. Much of the masterplan area has readings of nitrogen dioxide levels above 40 micrograms per cubic metre.

The South Poplar Masterplan seeks to address the impact of open space deficiency and poor air quality by creating equitable access to a new, green and sustainable network of open spaces.



Figure 38: Existing public realm and open spaces



## OPEN SPACE STRATEGY

The local plan's site allocations provide the following public open space requirements for each of the following areas:

- North Quay – 0.4 hectares
- Billingsgate – 0.4 hectares
- Aspen Way – 1 hectare

All sites within a relevant site allocation will be required to contribute towards the creation of public open space areas. Within heavily fragmented sites to the north of Aspen Way, open space contributions should be co-ordinated with neighbouring sites through the development management process to deal with pocket parks, stepped and ramped level changes, allowing continuous linkages of public space. To the south of Aspen way and sites with larger land holdings, generous areas of public open space will be expected to be provided to cater to a range of age groups and uses. The design of such spaces should enable users open views to the sky and to the waterfront.

The masterplan seeks to provide a series of new public spaces and landscaped routes which connect to the wider network of existing green spaces. A new north-south spine bridge over Aspen Way would connect South Poplar with Canary Wharf. A new east-west dockside promenade would link the popular West India Quay across to Trafalgar Way, Poplar Dock Marina and to the to-be-completed Wood Wharf Development. A series of public squares will allow large areas for residents, workers and visitors to congregate and enjoy the surrounds. These public squares should be linked with key connection routes as referenced in Guidance Section SP2 of this SPD.

New public spaces along the north-south and east-west routes will have a variety of characters ranging from the dockside promenade to an attractive bridge over Aspen Way. Smaller squares and pockets parks provide local places for physical activity, socialising, sitting, and dwelling and

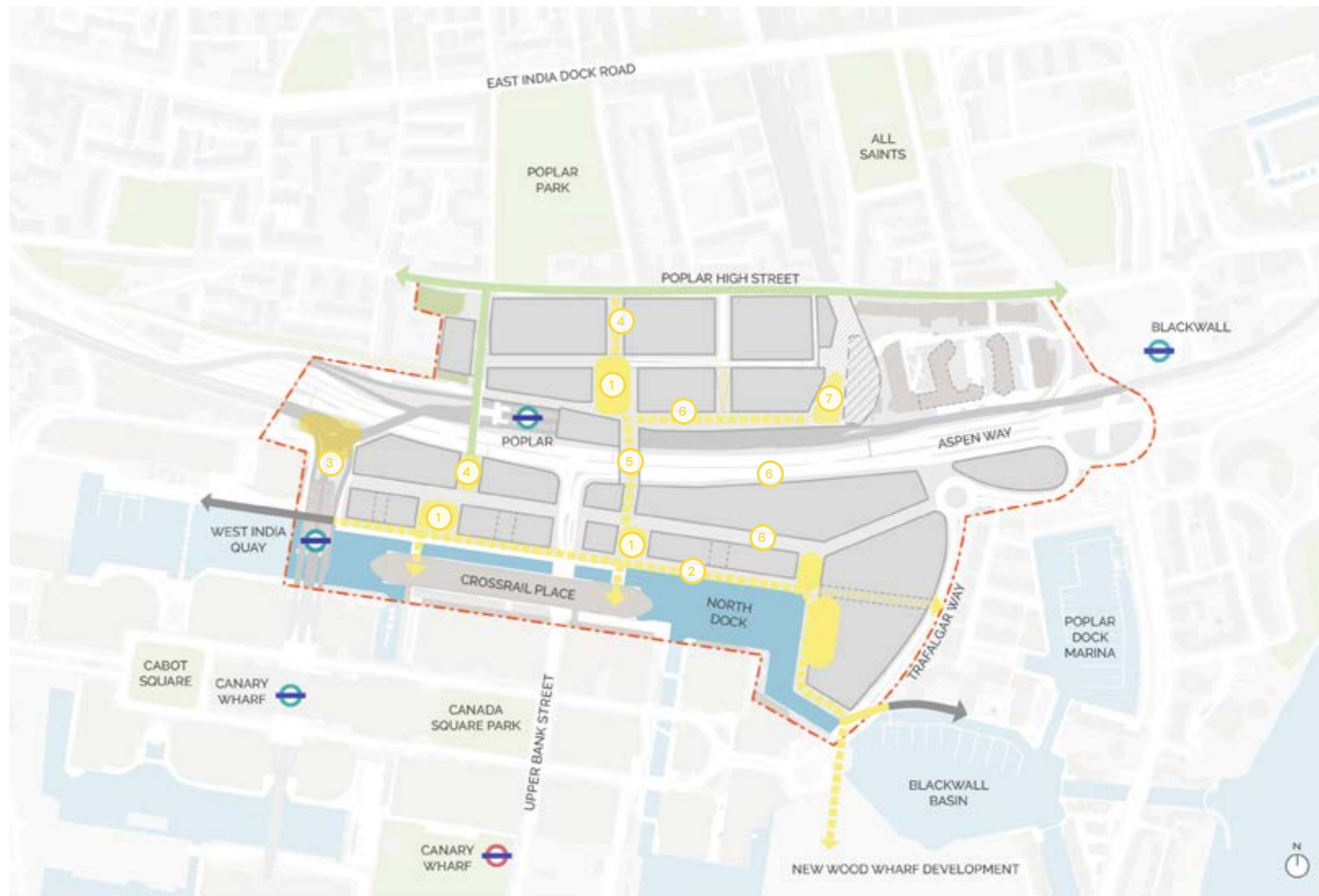


Figure 39: Public realm and open space strategy



allow for different activities throughout the day and evening. The placement of these new public spaces in relation to new buildings have been carefully considered to optimise daylight and reduce the impacts of noise and air pollution from Aspen Way.

**1 Public squares**  
 These significant areas of open space can mark the entrance to key public transports links including the Poplar DLR station and the Canary Wharf Elizabeth Line station. These spaces should be of high quality, well designed and at a human scale. Such spaces within the South Poplar area should be of a size and quality that is inviting to residents, visitors and workers to assist in facilitating social interaction and act as meeting points. Development will be expected to ensure linkages with the wider connections and routes whilst also providing a mixture of soft and hard landscaping features with public seating.

**2 Dockside promenade**  
 This will enhance the borough's important network of water spaces and also ensure that they are easily accessible. The continuous, attractive

waterfront space should be sufficiently generous to accommodate public and café seating, a promenade and maximise access to the water's edge. Buildings should be sufficiently setback from the water edge. This space should provide access for active use of the water where appropriate. On-water uses can help to activate the space, attract visitors to the area and reinforce a tangible connection to the dock's history. The waterside spaces should be designed to accommodate on-water uses where appropriate, for example floating cinemas and pop-up food markets.

**3 DLR underline**  
 Space underneath the existing DLR line can provide alternative open public space in an otherwise underutilised area. This area can be linked to the wider movement strategy. Such spaces should be secured as publicly accessible open space and/or to be used in an innovative way, while supporting the continued safe operation of the DLR. This will be considered as part of a wider borough initiative across the DLR network working with TfL and stakeholders and learning from precedents.

**4 Pocket park**  
 To negotiate complex level changes across the masterplan area ramps and steps can be used as part of developments to create smaller open spaces areas which are attractive and accessible.

**5 Aspen Way bridge connection**  
 The delivery of an additional and second crossing over Aspen way will provide a welcomed pedestrian and cycling link. This connection will be expected to be used primarily as a way to traverse across Aspen Way, however this area should be well planted and landscaped to create an appropriate buffer with Aspen Way.

**6 Internal streets**  
 Key connections and movement routes should be provided with shared surfaces with a pedestrian priority. Planting and seating should be used throughout.

The projected increase in population across the Isle of Dogs and South Poplar Opportunity Area will result in increasing pressure on the existing areas of open space. The delivery of high-quality additional areas of new open space is therefore

crucial to ensure healthy, sustainable and liveable neighbourhoods. The delivery of green and open space should be coordinated through the development management process. The locations of public squares and key open space areas are shown on Figure 39. These areas are either marked as indicative, under negotiation with relevant landowners or have been negotiated as part of specific ongoing planning applications.

The indicative location of these public open space areas may change, however the Council expects applications for development within the masterplan area to demonstrate that relevant proposals contribute to the site allocation requirements in a co-ordinated way. The detailed design of these public open spaces has been identified as a key priority in the delivery of the Masterplan and will also be shaped by the Council's development management process. In accordance with Local Plan policies contained within Chapter 13 (Enhancing open spaces and water spaces), development should not impact negatively on the quality of the existing and new open spaces such as negative impacts of wind funnelling and poor daylight/sunlight caused by overshadowing.



Figure 43: Dockside promenade precedent – Aker Brygge, Oslo



Figure 42: DLR underline precedent – Bentway Park, Toronto



Figure 41: Bridge connection precedent – Paleisbrug Bridge, Netherlands



Figure 40: There exist opportunities for additional public open spaces for example a landscaped park on the potential overbuild



## 4.4 MASSING AND HEIGHTS – SP4

The following local plan policies informed SP4 of the South Poplar Masterplan:

- Local Plan Policy Links**
- Policy S.SG1 Areas of growth and opportunity within Tower Hamlets
  - Policy S.SG2 Delivering sustainable growth in Tower Hamlets
  - Policy D.SG4 Planning and construction of new development
  - Policy S.TC1: Supporting the network and hierarchy of centres
  - Policy S.DH1 Delivering high quality design
  - Policy D.DH2 Attractive streets, spaces and public realm
  - Policy S.DH3 Heritage and the historic environment
  - Policy D.DH4 Shaping and managing views
  - Policy S.DH5 World heritage sites
  - Policy D.DH6 Tall buildings
  - Policy D.DH7 Density
  - Policy D.DH8 Amenity

In accordance with Tower Hamlets Local Plan policies, development should assist in defining the streetscape and street frontages. This will allow a more legible and permeable network of movements. To achieve this, the following supplementary guidance for development within the South Poplar Masterplan in relation to Massing and Heights is provided:

### SP4.1 Massing and Urban Blocks

1. Development should define and enclose the network of connections and spaces set out in SP2 and SP3 by:
  - a. Reflecting the illustrative masterplan massing on each site or in coordination with adjacent sites as depicted in Figure 46; and
  - b. Sustaining and enhancing heritage assets, their setting and their significance.



Figure 46: Illustrative masterplan





The illustrative Masterplan in Figure 46 depicts one way in which development can come forward. It provides a guide to give greater clarity for how design proposals for sites can respond to site specific elements while ensuring an appropriate level of consistency across the South Poplar area.

It is acknowledged that site constraints may present issues on individual sites, where this occurs development should coordinate proposals with adjacent sites to secure the delivery of the overall strategy. In delivering the massing, developments should ensure there is the ability to view the sky, docks and open spaces.

The massing of new developments should complement and provide a transition from the Canary Wharf Tall Building Cluster, particularly to the north of Aspen Way. It should ensure that buildings step down towards the dockside and open spaces.

**YOU SAID...**

Buildings should be designed for a human scale (Online Exhibition).



**WHAT WE DID...**

We included an expectation that tall building development, especially north of Aspen Way, will use a podium-plinth-tower structure, so that the public realm retains a human scale.

**SP4.2 Urban block structure**

1. Development is encouraged to provide an urban block structure consisting of three core components as depicted in Figure 47.

- a. Podium;
- b. Plinth; and
- c. Taller elements.

2. Podium and plinth massing should:

- a. Respond to the street sections set out in Figure 58;
- b. Respond to the surrounding context by:
  - Being proportionate in scale to the width of streets and adjacent public spaces;
  - Coordinating scale with neighbouring sites;
  - Decreasing in mass and scale away from One Canada Square and the Canary Wharf Tall Buildings Zone to the residential contexts of Poplar High Street and Trafalgar Way;
  - Mitigating impacts on residential amenity;
  - Providing a human scale sense of enclosure;
  - Supporting the activation of new open space areas.

- c. Create articulation and visual interest to deliver a varied and coherent urban streetscape; and
- d. Clearly and consistently define street edges, corners and public open spaces.

3. Podium and plinth design should:

- a. Deliver active frontages;
- b. Clearly define and deliver residential entrances along relevant residential active frontages, responding to the indicative layout in Figure 6g;

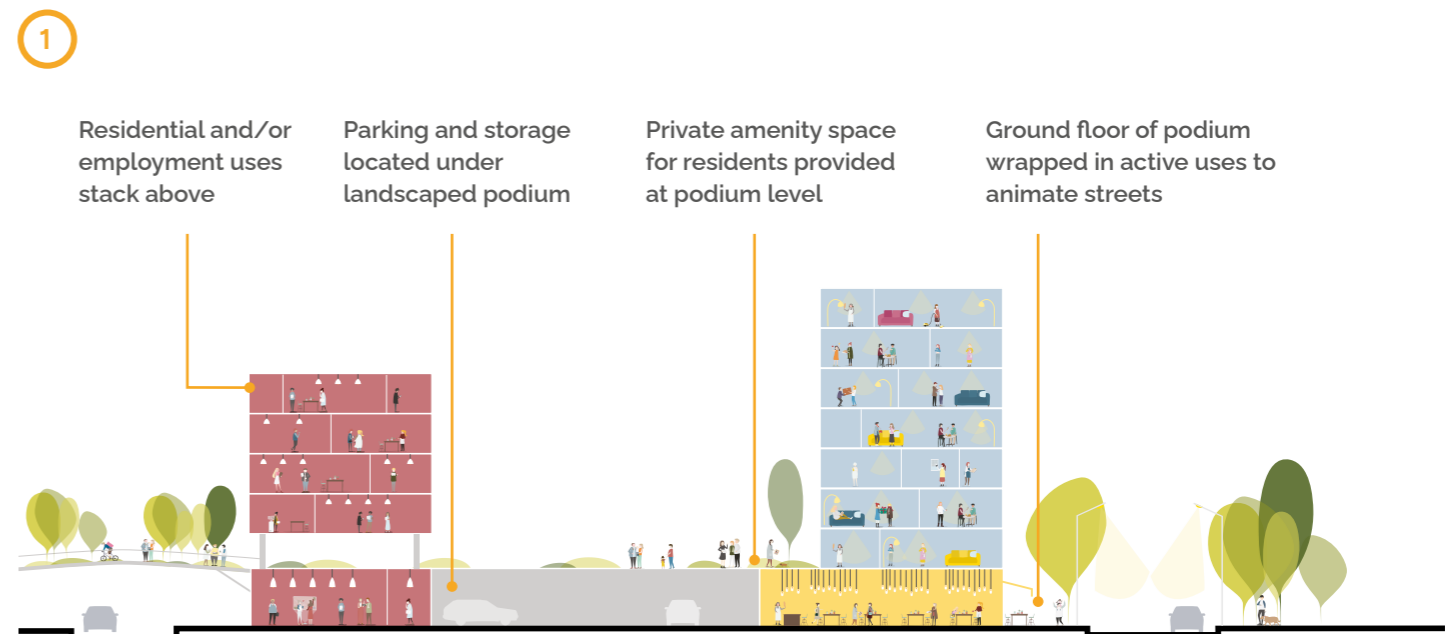


Figure 47: Indicative section through an urban block structure incorporating podium, plinth and taller elements

- c. Conceal servicing, parking and delivery arrangements through well-designed layout and façade treatment.
4. Taller element massing and design should:
- a. Step down from the central location of One Canada Square and towards the edge of the Canary Wharf Tall Building Zone (if located within the Canary Wharf Tall Building zone);
  - b. Step down from Canary Wharf and Blackwall Tall Building Zones, avoid the merging of clusters and be sensitive to the height of surrounding contexts (if located outside a Tall Building Zone);
  - c. Accord with the latest Civil Aviation Authority heights guidance for the London City Airport.

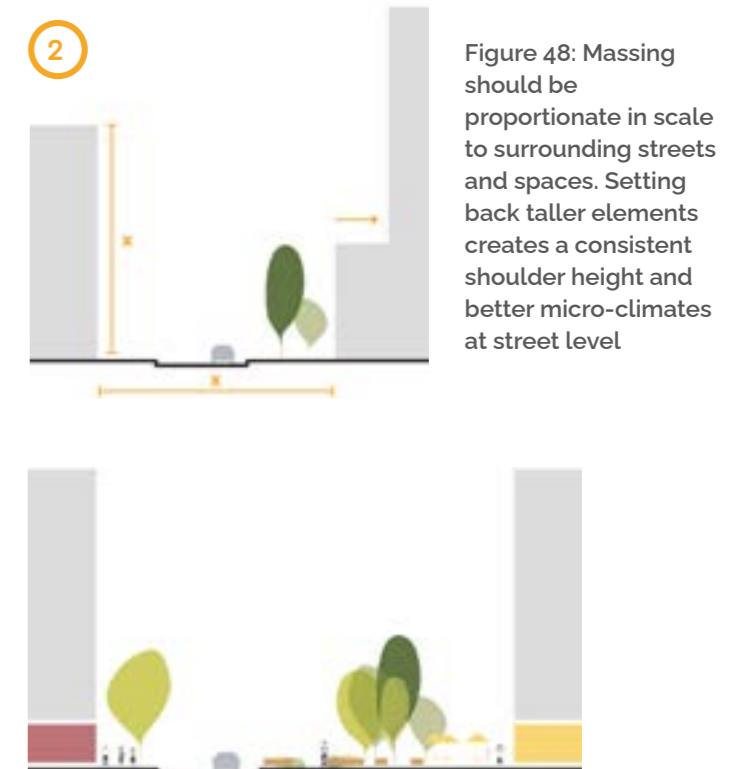


Figure 49: Provide active uses adjacent new open spaces that support activity and create visual interest

## URBAN BLOCK DESIGN

### PRINCIPLES

London Plan Policy D9 provides a framework with which to develop policies and guidance around tall buildings and to assess applications for tall buildings. This section provides additional guidance to support that policy as well as the relevant local plan policies, focusing specifically on the character and built form of South Poplar.

The provision of a well-defined and legible urban block structure can address the existing poorly defined movement network across the masterplan area. In appropriate locations as shown in the illustrative masterplan in Figure 46, an urban block network has been identified to assist in providing a more legible and permeable network of movements.

An urban block should consist of the following three elements:

- Podium;
- Plinth; and
- Taller element(s).

An urban block structure has a number of associated benefits and is favoured over a standalone building or tower. An urban block provides a sense of enclosure to streets and defines the street network. It allows for a mix of typologies and uses, with clearly defined active street edges and frontages on all sides. An urban block can also mitigate the wind impacts of tall buildings and servicing arrangements including cycle stores, waste and plant rooms can conveniently be placed within a podium.



Porters Edge, Canada Water

Taller elements located opposite to reduce overlooking and improve sunlight and daylight



Het Kasteel, Amsterdam

A consistent shoulder height defines the streetscape. Residential units can enjoy dual aspect looking out on the street and courtyard



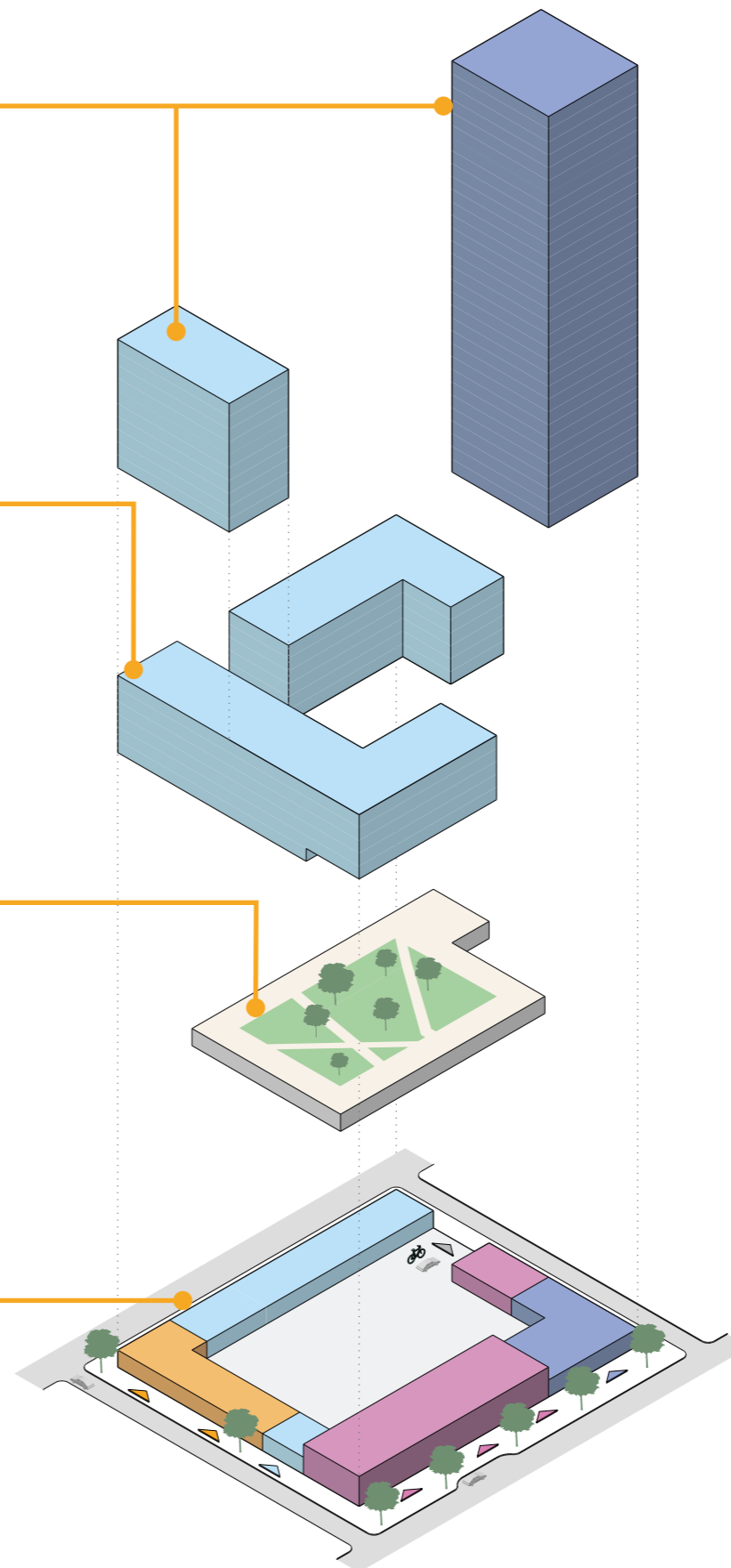
St Andrews, Bromley-by-Bow

Courtyards at podium level can provide private, landscape amenity space for residents stacked above parking, bins, and service areas



Royal Albert Wharf, Newham

Animate streets through active uses at ground floor. Along residential streets, homes should be accessed directly from the street and provide passive surveillance



#### Taller elements

Taller elements can potentially include tower structures and/or taller parts of a plinth component. These will need to be designed in accordance with the guidance set out in SP4.3 of this masterplan and local plan policies.

#### Plinth

A plinth can appear visually the same as a podium, but with distinct roles in structuring the urban environment. For the South Poplar Masterplan, a consistent shoulder height is promoted to ensure a sense of human scale with varied uses and active frontages.

#### Podium

A podium is a platform of approximately one to three stories that usually fronts and defines the street. A tower then can sit above this, set back from the edge of the podium so the tower and podium read as two distinct forms. The top of the podium can typically include commercial activity, outdoor communal amenity space and/or play space.

Figure 50: Illustrative urban block



In accordance with Tower Hamlets Local Plan policies, development should ensure an aesthetically balanced skyline that responds appropriately to the Canary Wharf Skyline of Strategic Importance. Development should contribute positively to the local economy and environment. To achieve this, the following supplementary guidance for development within the South Poplar Masterplan in relation to Massing and Heights is provided:

**Sp4. 3 Heights, views and skyline**

1. The development of tall buildings and taller element massing and design should:
  - a. Respond to the proposed height strategy depicted in Figure 56;
  - b. Ensure a varied and dynamic skyline with visual layering and breaks in between buildings to allow views through to the open sky;
  - c. Ensure a stepping down of building height from the height peak at the centre of the Canary Wharf skyline of strategic importance;
  - d. Provide an incremental transitioning of building height down to the residential scale of South Poplar towards Poplar High street and Trafalgar Way;
  - e. Design tall buildings with an identifiable top, middle and bottom through detailing or materiality; and
  - f. Avoid a solid wall of development with height differences and breaks between buildings, particularly adjacent to Aspen Way
2. In addition to meeting the requirements of part 1 of this policy and responding to the design principles depicted in Figures 52-55, proposals for tall buildings outside of a tall building zone and/or that do not conform to the stepping down principle as shown in Figure 56 should:

- a. Address the provision of key physical and social infrastructure identified within this masterplan in Section 5 (Infrastructure Phasing and Priority Plan);
  - b. Improve the legibility of the area by marking a significant location associated with the provision of physical or social infrastructure or local asset; and
  - c. Demonstrate that the prominence of One Canada Square and the Canary Wharf Skyline of Strategic Importance would not be undermined.
3. Development applications for tall building proposals should be submitted with verified daytime and evening/night-time views from the following Borough designated views and views from the London Views Management Framework:
    - a. View from the bridge over Regent's Canal on Ben Johnson Road to St Anne's Church and Canary Wharf in the background;
    - b. View from Salmon Lane to St Anne's Church and Canary Wharf in the background;
    - c. View from Langdon Park to Balfour Tower and Canary Wharf in the background;
    - d. View from East India Dock Road to Balfour Tower and Canary Wharf in the background; and
    - e. View from the General Wolfe statue in Greenwich Park along the axis of Maritime Greenwich world heritage site to St Anne's Church with Canary Wharf in the backdrop.

**HEIGHT DESIGN PRINCIPLES**

London Plan Policy D9 provides detailed guidance for assessing tall building developments. Among the considerations within the policy is the impact of new tall buildings on existing skylines. This section provides additional detail to assess the impact of new tall buildings in this area on long-range, mid-range and immediate views by setting out the key landmarks in the area and the existing structure of the skyline within the area. It also presents the existing spatial hierarchy of tall buildings to ensure that new development can reinforce that hierarchy.

Building heights within the masterplan area should mediate between the height peak at One Canada Square in Canary Wharf and the lower scale context of South Poplar. Building heights and massing should be varied to provide an articulated skyline that responds to the Canary Wharf context but also provides a positive urban relationship to South Poplar and Trafalgar Way. Building proportions and urban grain should also create a transition between the predominantly larger floorplate office blocks of Canary Wharf and the finer grain of South Poplar which is characterised by 3-6 storey post-war housing. The masterplan's heights strategy has been prepared in this context.

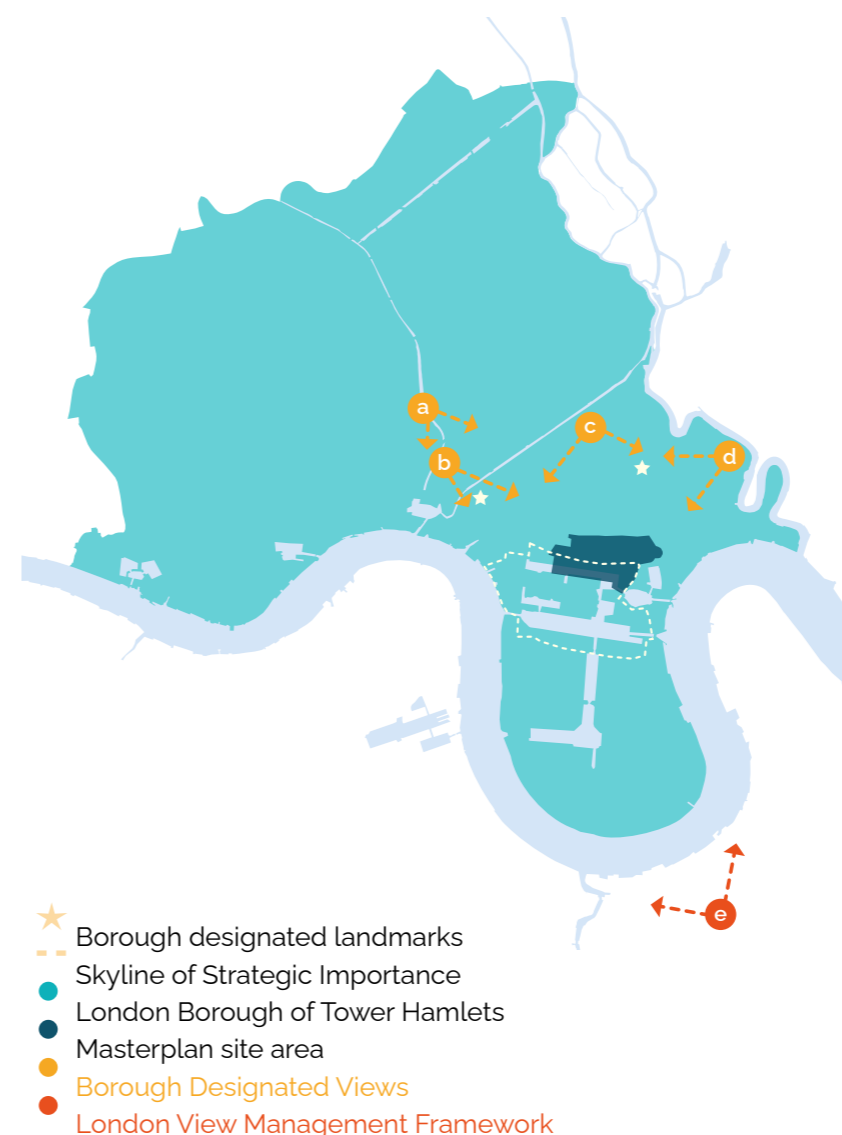


Figure 51: Borough designated view and London View Management Framework

**YOU SAID...**

The views south towards Canary Wharf are important for the character of South Poplar and should be preserved as much as possible (Online Exhibition).

**WHAT WE DID...**

We developed guidance to ensure that development respects the existing skyline of Canary Wharf and the height peak at One Canada Square.

The development of tall buildings can provide many positive contributions to the community and wider borough. The densities provided in tall buildings can deliver significant amounts of new homes, employment floorspace to support local jobs and other valued community uses to support healthy and liveable neighbourhoods. However, the design and location of tall buildings can also cause considerable harm to the character and identity of an area through individual or cumulative developments. Parts of the masterplan area fall within one of two Tall Buildings Zones (Canary Wharf and Blackwall Cluster), within which tall buildings are considered appropriate as set out in the Tower Hamlets Local Plan.

Local Plan Policy D.DH6 identifies Canary Wharf as a skyline of strategic importance and expects new development to respect that skyline and its iconic appearance. It also expects development to step down from the height peak at One Canada Square. This policy was developed to ensure that the borough maintains a coherent, legible skyline. This is consistent with London Plan Policy Dg, which explains the role that tall buildings can play in legibility, signifying locations that are important for civic, commercial and transport hub reasons. The character to the north of Aspen Way is largely defined by mid- and low-rise buildings, and a particularly prominent historic environment. Tall buildings have the potential to undermine that character if they are located or designed insensitively.

Stark transitions between tall and low-rise buildings can have a jarring effect. The existing relationship between the lower-rise character of South Poplar and the high-rise character of Canary Wharf is somewhat mediated by the infrastructure corridor, including Aspen Way. As that corridor loses its impact through new development, it will be important to ensure that the transition between the two areas, and the two characters, is gradual.

If tall buildings are proposed in parts of the masterplan outside of designated tall building zones and/or that do not conform to the general

stepping down principle as shown in Figure 56, the location and design of such proposals must be carefully considered. Outside of designated tall building zones, such proposals should be incorporated into urban blocks with the position of taller elements set back to create a consistent shoulder height.

In the consideration of applications for tall buildings outside a designated tall building zone and/or that do not conform to the general stepping down principles set out within the masterplan's height strategy, the guidance has been prepared to provide further information as to how to respond to Local Plan Policy D.DH6.

Proposals will need to demonstrate how strategic infrastructure provision would be unlocked to address deficiencies and future needs. Section 5 (Infrastructure Phasing and Priority Plan) outlines a range of social and physical infrastructure items which will assist in delivering the vision of the South Poplar Masterplan. It must be demonstrated that the provision of such infrastructure would not otherwise be possible without the development of the tall building proposal. Additionally, such tall building applications would need to serve as landmarks, identifying a significant location in the context of a strategic infrastructure provision or of a local asset.

Applications must not undermine the setting of, and views towards, One Canada Square. One Canada Square, whilst not a listed building, is a borough and London-wide landmark with a prominent role. The building has been seen as an important symbol of late-modern/post-modern architecture in London and throughout the world for over 30 years and forms the pinnacle of the dramatic and significant skyline of Canary Wharf, itself being visible over a wide area. The tower represents the start of a transition from the modernist and high-tech architecture popular in the late 20th century - and especially the 1980s development boom - to the sculptural, post-modern architecture of the early 21st century. It sits, alongside 30 St Mary Axe (the Gherkin) and

the London Eye as one of the most prominent and globally recognisable symbols of contemporary London. One Canada Square is visible in a number of borough designated views and within the London View Management Framework. Any tall buildings in these views must be subservient to One Canada Square and be managed so that the silhouette of the building is considered in the skyline. Views to the iconic pyramid top of the building must not be significantly undermined by any new development.

Tall building proposals will need to satisfy the requirements of Local Plan policy D.DH6. The Council is currently undertaking a separate policy project in relation to the development of a Tall Buildings Supplementary Planning Document (SPD). This document seeks to guide the design and planning of tall buildings across the borough and will contain the most up-to-date borough guidance on the development of tall buildings. Further information on the project can be found here: <https://talk.towerhamlets.gov.uk/tallbuildings>.

Tall building massing and height must be delivered below the Civil Aviation Authority's Safeguarded and Obstacle Limitation Surfaces guidance for London City Airport. This is relevant for both the construction and operational phases of the building.

In addition to the identified views, submitted skyline related visualisations should incorporate existing and consented development and where appropriate emerging proposals to demonstrate the impact on the skyline and setting of heritage assets. Tall buildings heights should also accord with the latest Civil Aviation Authority heights guidance for the London City Airport.

It should be noted that this guidance section seeks to present an urban design driven strategy for tall buildings. It is intended to support the existing design policies and does not take account of site capacities or viability. Capacity and viability on individual sites will be assessed as part of applications for planning permission and are beyond the scope of this SPD.

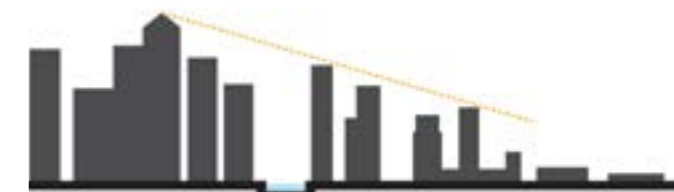


Figure 52: Step down from One Canada Square while avoiding an overly linear or terraced approach



Figure 53: Create a varied and dynamic skyline with visible breaks between buildings and avoiding a terraced approach to transitioning from Tall Building Zones to lower scale context



Figure 54: Create an identifiable top, middle, and bottom for tall buildings through detailing or materiality

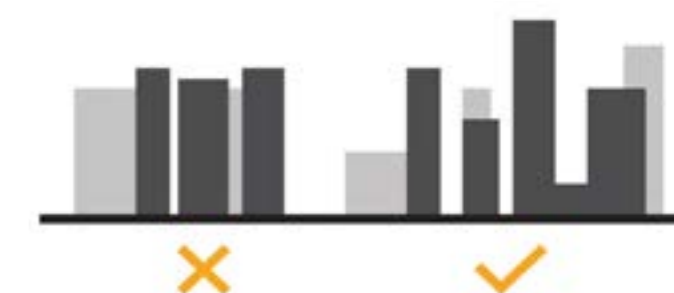


Figure 55: Adjacent buildings should have height differences of at least 20m. Varied heights help to provide clear breaks between buildings and avoid a solid wall of development, particularly next to Aspen way



## HEIGHT STRATEGY

The drawing opposite shows the proposed height strategy across the site. The height strategy mediates between the cluster of tall buildings located in Canary Wharf and the lower-rise residential context of Poplar to the north. Buildings heights should step down from One Canada Square (240m) to contribute to the existing townscape of Canary Wharf and its recognisable and iconic Skyline of Strategic Importance.

Figure 57 below shows the predominant building heights within and adjacent the masterplan area as well as heights of consented schemes.

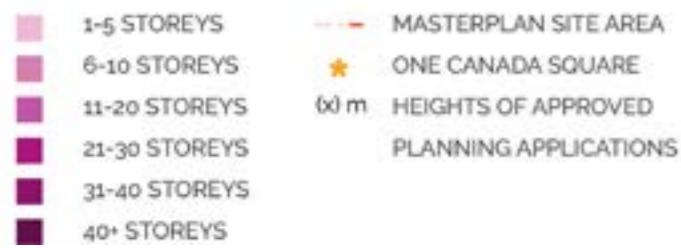


Figure 57: Existing and approved building heights

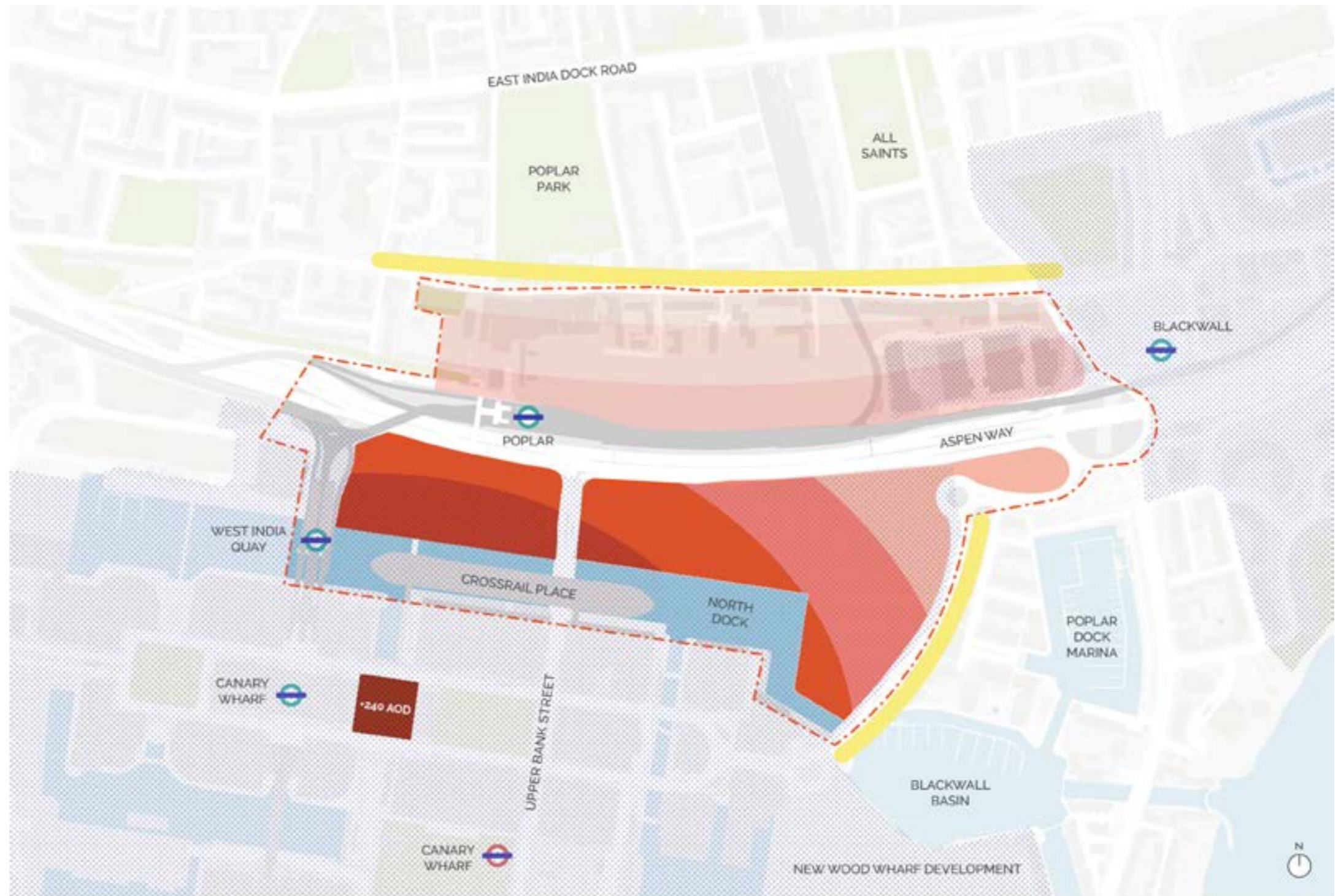


Figure 56: Illustrative height strategy



### Building heights

The following illustrative diagrams demonstrate an approach to building heights along key sensitive edges.

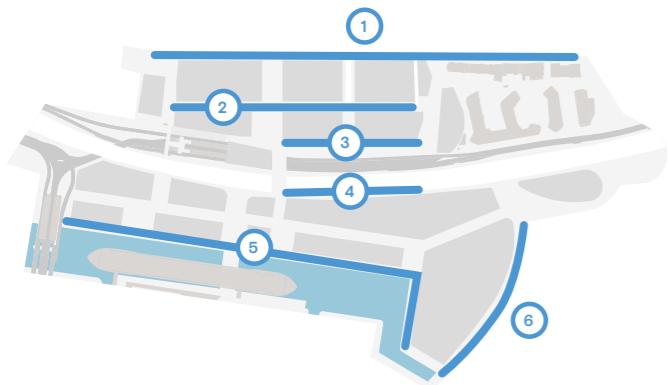


Figure 58: Illustrative approach to height along key edge conditions



#### 1. POPLAR HIGH STREET

New development should reinforce the predominantly 3-6 storey existing context and create a consistent building line fronting the street.

#### 2. SIMPSON ROAD

New development along Simpson Road should provide a transition to Poplar High Street. 2-3 storey steps can help create a transition.

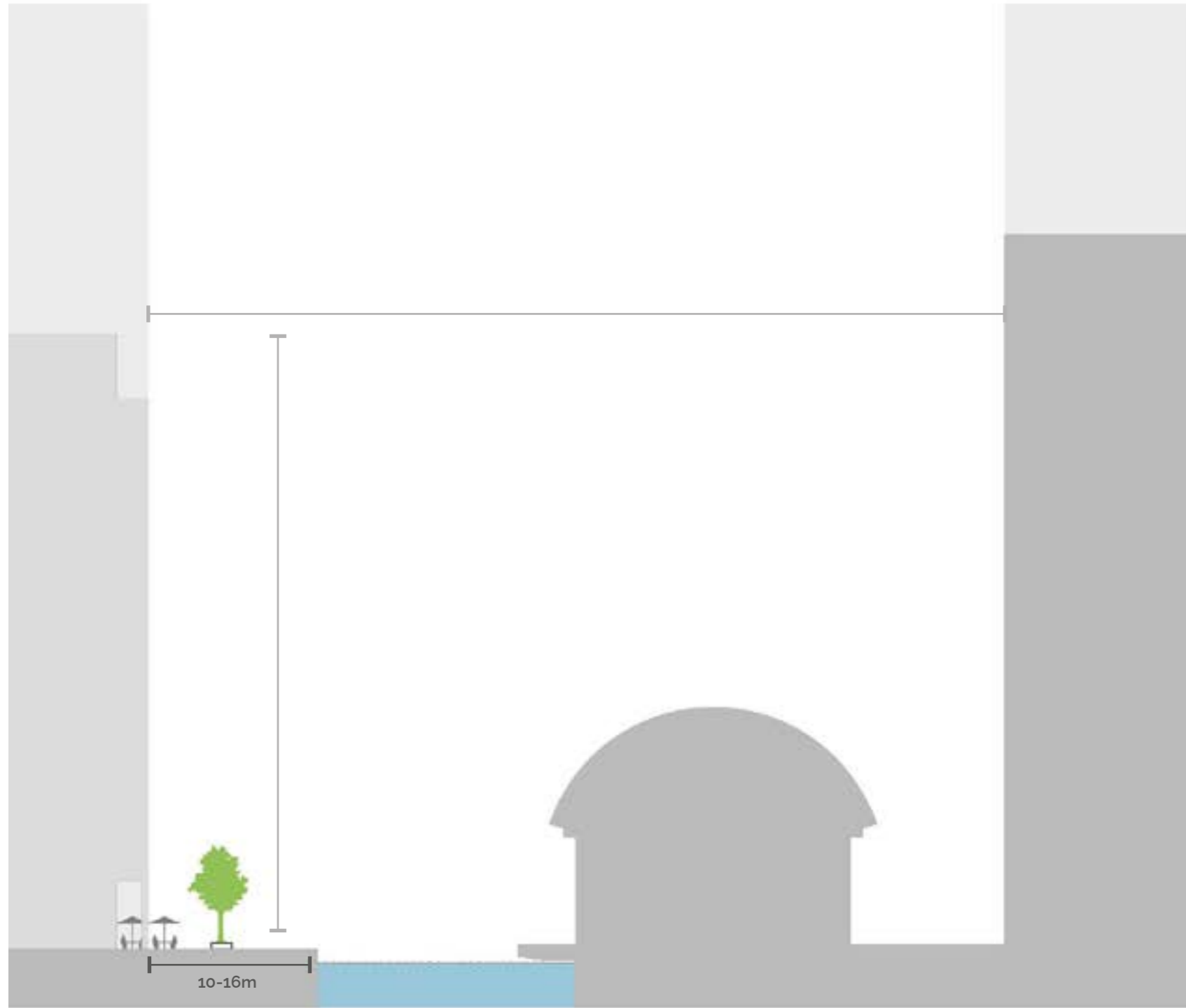
#### 3. DEPOT ROAD

Heights should generally step down across the depot site towards Poplar High Street. However, heights should vary to create a dynamic skyline

#### 4. ASPEN WAY

Taller elements are appropriate within the Tall Building Zone of Canary Wharf but should step down towards Poplar and avoid the merging with the Blackwall Tall Building Zone. Heights should vary across the site and avoid creating a wall of development to Aspen Way.





## 5. DOCKSIDE

Buildings adjacent key open public spaces along the dockside should step down to relate to the scale of North Quay and the dockside

**YOU SAID...**  
Buildings should come down to meet the existing character along Poplar High Street.



**WHAT WE DID...**  
We set out a height strategy that supports a general stepping down from Canary Wharf to Poplar High Street.



## 6. TRAFALGAR WAY

Heights should vary along Trafalgar way to avoid a consistent wall of development and transition to the adjacent medium density residential context. Buildings adjacent the Grade II listed Accumulator Tower should create a positive frontage to the heritage asset

## 4.5 USES AND SOCIAL INFRASTRUCTURE – SP5

The following local plan policies informed SP5 of the South Poplar Masterplan:

### Local Plan Policy Links

- Policy S.SG1 Areas of growth and opportunity within Tower Hamlets
- Policy D.SG4 Planning and construction of new development
- Policy S.SG2 Delivering sustainable growth in Tower Hamlets
- Policy S.H1 Meeting housing needs
- Policy S.DH1 Delivering high quality design
- Policy D.H2 Affordable housing and housing mix
- Policy D.H3 Housing standards and quality
- Policy S.EMP1 Creating investment and jobs
- Policy D.EMP2 New employment space
- Policy S.TC1 Supporting the network and hierarchy of centres
- Policy S.CF1 Supporting community facilities



### DID YOU KNOW

The London Plan (Policy H4) sets out measures to ensure that public sector land delivers at least 50% affordable housing on each site.

### SP5

In accordance with Tower Hamlets Local Plan policies, development should provide a vibrant mix of uses that meets the needs of local residents, students, visitors and workers. Active uses should be delivered to ensure appropriate activation of the area and integration between the street and building. To achieve this, the following supplementary guidance for development within the South Poplar Masterplan in relation to Uses and Social Infrastructure is provided:

1. Development should:

a. Deliver uses with regard to the indicative designations depicted in the Masterplan Use Strategy depicted in Figure 64 including::

- Residential uses;
- Employment and supporting uses; and
- Community and education use.

b. Delivering active frontages with regard to the indicative designations depicted in the Active Frontage Strategy depicted in Figure 69 of

- Primary active frontages;
- Secondary active frontages; and
- Residential active frontages.

The delivery of a mix of uses will support an increased population across the masterplan area and foster a vibrant, mixed and cohesive neighbourhood. The three broad designations within the masterplan use strategy are detailed more opposite.

## USE STRATEGY

The masterplan site occupies a unique location bordering Canary Wharf and South Poplar. Canary Wharf is an international centre of business and finance although in recent years there has been increasing diversification with new housing, leisure, and community facilities. South Poplar by contrast is a vibrant, predominantly residential neighbourhood with a strong, multicultural community. Against the contrasting environments of South Poplar and Canary Wharf is the need to deliver high quality, and truly affordable housing, provision of jobs and supporting community services.

The site and immediate context continue to experience unprecedented growth and development pressures. This masterplan will help manage growth sustainably, secure benefits for the wider community, and ensure supporting infrastructure is delivered alongside new housing. New uses must also compliment the characters of both South Poplar and Canary Wharf and create a transition in scale between the iconic skyline of Canary Wharf and the low-rise, historic Poplar High Street.

South of Aspen Way, development should contribute to the Metropolitan Centre of Canary Wharf. New uses and facilities should contribute to a vibrant mix of housing, employment, retail and leisure. Different types of workspace should also be provided including modern offices, studio spaces, incubating emerging business, and industrial workspace. The area north of Aspen Way is a vibrant, residential community and a suitable location for housing (including family homes), social infrastructure, employment floorspace and low to mid-rise new mixed-use development. Across the masterplan area, the provision of a minimum 10% affordable workspace should be provided (in accordance with the Tower Hamlets Local Plan).

### Residential uses:

Tower Hamlets Council has been set a housing

target by the Greater London Authority in order to maximise the supply of homes to meet local and strategic needs. Across the Borough, this includes a target of delivering 58,965 additional homes by 2031. 31,209 (57%) of these homes are targeted to be delivered within the Isle of Dogs and South Poplar area. Local Plan policy S.H1 (meeting housing needs) sets out a range of policies to ensure developments achieve these targets within the specified timeframes. Within the South Poplar Masterplan Area, development is expected to contribute to the sub-area and borough wide housing targets in order to ensure the needs of the wider population are met.

North of Aspen Way, there is an existing, predominantly residential community with a strong identity. Across this area, there is an existing high proportion of affordable housing units serving the community. This area is considered appropriate for significant housing delivery as part of a wider mixed-use area. In these locations, given the strategic context and location, high density residential schemes would be suitable to assist in meeting the Council's extensive housing targets. The provision of residential uses in this area should be integrated well with the existing educational and community uses that exist at the New City College and the East End Community Foundation. The high proportion of affordable housing units in this area should be maintained as part of forthcoming developments given the public ownership of a number of sites. Where more than one use is identified within the masterplan use strategy for a particular site, development should consider mixed use buildings and/or the delivery of alternative uses depending on the financial and/or commercial viability at the time an application is submitted.

South of Aspen Way, sites including North Quay and the Billingsgate Market lie within the Borough's Secondary Preferred Office Location which is a designation prioritising the delivery of office led uses or those that are within the scope of the Central Activities Zone (CAZ) function. Within this designation, Local Plan policy S.EMP1 sets a 25% maximum site threshold limit on the provision of



residential floorspace. This threshold amount can be exceeded subject to robustly demonstrating that it would not be viable to deliver the required office/CAZ led uses and subject to the provision of evidence detailing that the supply of sufficient employment capacity to meet future need would not be compromised. Where proposed residential offers within these site locations seek to exceed the maximum 25% policy threshold limit, development should meet the local plan application requirements as relevant.



Figure 59: Royal Albert Wharf is a new, residential mixed-use development with residential uses stacked above active uses including artists studios, commercial space and cafe. Development along the North Quay dockside should similarly provide an active uses at ground floor.

Residential uses in this area should be oriented towards the south, facing North Dock. This location positions residential uses away from the northern edge of Aspen Way. Given the high levels of traffic running through this motorway, there are higher levels of air pollution and heightened impacts of noise. Given this context, residential uses should be positioned to the south of sites to take advantage of the waterfront whilst also positioning future residents away from this harsh edge. It is noted that a certain degree of flexibility is required to allow the delivery of a successfully viable scheme. If an application seeks to position residential uses towards the north of these sites contrary to the masterplan use strategy, it must be demonstrated that the residential amenity of future occupants would be at an acceptable level in reference to air quality, noise and other residential amenity measures.



Figure 64: Illustrative use strategy

- |     |                                |   |   |
|-----|--------------------------------|---|---|
| --- | MASTERPLAN SITE AREA           | □ | INDICATIVE URBAN BLOCK                        |
| ■   | RESIDENTIAL                    | ⋯ | POTENTIAL BREAK IN BUILDING LINE              |
| ■   | EMPLOYMENT AND SUPPORTING USES | ▨ | POTENTIAL ADDITIONAL OVERBUILD AREA           |
| ■   | COMMUNITY / EDUCATION          | ⊃ | POTENTIAL ADDITIONAL URBAN BLOCK              |
| ■   | SECONDARY SCHOOL LOCATION      | ▨ | POTENTIAL LOCATION FOR URBAN FREIGHT SOLUTION |



### Employment and supporting uses:

In addition to meeting the needs for new homes across the borough, Tower Hamlets has a significant role to play in London's global economy. Canary Wharf to the south of the masterplan area is home to some of the world's largest financial and professional service organisations. The total number of jobs in the borough is projected to rise from 285,600 in 2015 to 410,600 in 2031. Within the Isle of Dogs and South Poplar sub-area, the Mayor of London's OAPF for this area projects that 110,000 jobs are expected by 2041. Given this context, the provision of suitable employment floorspace to accommodate this growth is crucial to ensuring these ambitious targets are met.

In addition to the provision of and demand for office space, there is a pre-existing and emerging culture of entrepreneurialism amongst local residents with an increasing demand for employment space to service small family companies, creative enterprises, start-ups, light industrial manufacturers and small to medium enterprises.



Figure 60: Second Home in London Fields is a mixed-use co-working space. Clearly contemporary but in keeping with the street character, the building is a successful precedent for how alternative employment space the could contribute to the existing community hub on Poplar High Street.

The borough's town centres and activity areas play an important role in defining a place's identity and character. The makeup of which can provide an anchor to a local neighbourhood and bring together the borough's diverse communities. As these communities have developed, town centres and activity areas have become hubs for leisure,

social and community uses. It is important that town centres and activity areas remain vibrant, sustainable and flexible to meet the needs and changing demands of the local residents they serve.

Much of the masterplan area is within a local plan designated Activity Area. North of Aspen way, development should provide a transition between the scale, activity and character of the Canary Wharf Metropolitan Centre and the neighbourhood centre at Poplar High Street, and support a mix of uses which make a positive contribution to the health and well-being of residents. The DLR Depot has a continuing role in terms of employment in the area in supporting DLR operations and ongoing maintenance which will be an ongoing requirement.

Within the character areas of the Station gateway, Blackwall Edge and St Matthias area, development should contribute positively to the function, vitality and viability to the Poplar community by delivering employment and supporting uses in accordance with the following:

- Provision of retail and leisure floorspace;
- Support the evening and night-time economy – contributing to inclusivity, economic vitality and viability;
- Provision of a mix of unit sizes and types to assist in creation of vibrant centres that offer a diverse level of choice;
- Support multi-functional, diverse and inclusive leisure and cultural venues;
- Promote local enterprise and contribute to local character;
- Support temporary and community uses to help activate units and sites;
- Provision of local creative spaces, maker, doer spaces to support the emerging creative industry. Such provision would also attract people from Canary Wharf, the wider Isle of Dogs area and across the borough.

South of Aspen Way, a significant amount of office floorspace should be provided in accordance with local plan policies. Larger building floorplates are likely to be required to accommodate larger

businesses with specific building design needs in line with the urban block structure identified in SP4 of this SPD. As per the Local Plan's Secondary Preferred Office Location designation for much of this area, land uses will be predominantly office and strategic CAZ use led unless otherwise justified in accordance with local plan policies.

### Community Uses

While this SPD does not designate specific land uses for the masterplan's community function - including GP surgeries and dentists' offices - as this would be beyond the scope of this document, these uses are considered appropriate in all areas designated for community and/or supporting uses. These facilities are encouraged to come forward where there is appropriate demand and consistent with local plan policies.

The Tower Hamlets Infrastructure Delivery Plan (IDP) assesses future need for social and community infrastructure and plans for its provision. More information regarding planned facilities across the borough can be found in the IDP. Planned facilities proximate to South Poplar include an expansion of the Aberfeldy Practice to the northeast of the SPD area and a new health facility to be provided as part of the Wood Wharf development to the southeast of the SPD area.

### Education Provision

The Council's Local Plan allocates the Billingsgate Market site for the provision of a secondary school. This allocation was undertaken with a robust evidence base, including a Site Allocations Methodology (2018), Site Allocations – Sequential and Exceptions Tests (2017), Spatial Assessment Need for Schools (2018) and a Local Plan Viability Assessment (2018).

Work undertaken through the South Poplar Masterplan SPD and through joint working with the Council's Education Department, has identified the south-eastern part of the site would be most appropriate to host an educational use. This location ensures this sensitive use would be located away from the harsh Aspen Way edge.

Whilst the provision of a secondary school has been allocated within the Local Plan on the site, a flexible approach is considered appropriate in this instance to ensure the provision of a facility to meet the needs and demands of the borough at the time a development application is submitted. This approach would also ensure that land within the site is used efficiently. The provision of a secondary school has been tested as part of this SPD. As per Figure 61 the provision of a secondary school could reach up to 1.75ha. The site landowner(s) are expected to work co-operatively with the Council to determine the need, type, size and design of the educational facility as part of the development management process. Further details on the funding and delivery of the educational facility on the site is provided within the Infrastructure Phasing and Priority Plan (page 57).

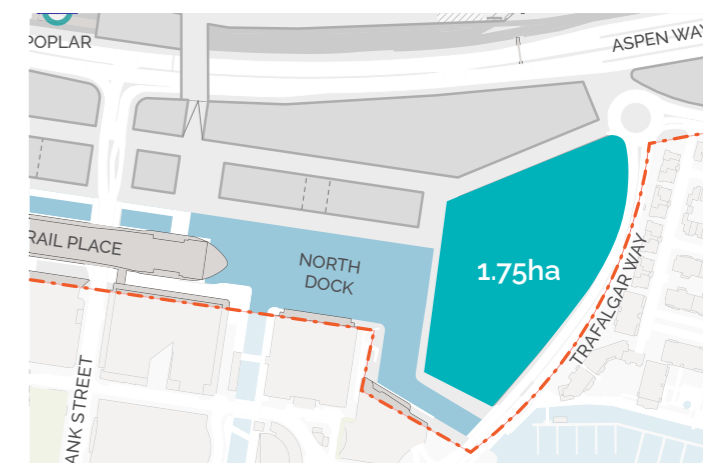


Figure 61: Approximate area required for a secondary school



Figure 62: The Chelsea Academy is a successful example of a secondary school with 6th form provision on a compact site of 0.7ha in a dense, urban environment. Playing pitches have been creatively incorporated on the building's roof.



It is important for local residents to continue to have access to high quality and relevant post-16 education. The Poplar campus of New City College (NCC – formerly known as Tower Hamlets College) is within the masterplan area and is the largest provider of post-16 education in the borough. NCC has aspirations to redevelop its existing campus to enhance the diversity and quality of its educational facilities. It is anticipated that improvements to the college would be delivered as part of a mixed-use development including a high density residential provision.

### Urban Freight Solution

The Isle of Dogs and South Poplar: Opportunity Area Planning Framework (OAPF – GLA 2019) designates the Billingsgate Market site as a potential location for a freight use. This designation is in recognition of the considerable congestion issues across the Opportunity Area and significantly poor air quality. The OAPF sets out to encourage freight sustainability and consolidation, calling for innovative solutions to encourage co-ordinated servicing and delivery.

The concept of a freight solution within an urban context is promoted by local authorities primarily as a means of reducing delivery vehicles within a specified area. This can subsequently support:

- Reduction in vehicle kilometres as a result of consolidated loads with reduced fuel consumption and vehicle emissions;
- Improved local amenity with reduced noise intrusion and better air quality;
- Reduced road danger with a reduction in number of vehicle trips;
- Reduced congestion, allowing space to be prioritised for cyclists and pedestrians;
- Improved commercial and operational efficiency with fewer resources used to transport larger volumes.

An urban freight initiative can provide solutions for supply chains, procurement and infrastructure and can come forward in various ways including as a last-mile delivery and/or servicing hub. A last mile

delivery hub is a logistical, servicing and delivery solution comprising the last part of the journey in the movement of goods from its originating location to a final destination. Such goods would be held on-site within a last-mile delivery building for a short period of time, before being transported to its final destination using more sustainable methods including e-cargo bikes or with smaller, more efficient vehicles.



Figure 63: A proposal in Ilderton Road, Southwark for a local storage and distribution warehouse wrapped with smaller workspaces to create active edges with residential uses above. An internal loading yard mitigates noise. The total site area is approximately 0.4ha and is an indicative illustration of the size of any future potential use.

Work undertaken through the South Poplar Masterplan SPD process has further explored the potential for an urban freight solution as recommended by the OAPF and considered how such a use could be integrated on the Billingsgate Market site successfully. An urban freight solution would be classified as an 'Employment and Supporting' use for the purposes of the illustrative masterplan use strategy. An illustration of a potentially suitable location for an urban freight solution use is shown on Figure 64 to examine how the use could be integrated into the masterplans' proposed urban block hierarchy as part of a podium structure. Such an arrangement would allow the provision of residential and/or

commercial uses to be located above, ensuring minimal interruption to surrounding uses.

On the Billingsgate Market site, such a use could potentially be located to the north-east of the site, utilising vehicular access off Trafalgar Way. This location towards the Aspen Way frontage would ensure such a use which is of a comparatively low employment density would interface with the harsher air and noise quality environment in place of other sensitive (residential, educational) uses which would otherwise not be appropriate.

The scale, location, capacity and type of a potential urban freight solution on the Billingsgate Market site and/or wider masterplan area should be established first through an overarching freight and logistics strategy to better understand the needs and demands within the area. The strategy would be led by the Council, TfL and the GLA with support from relevant landowners and additionally would assist in understanding how to ensure urban freight is an attractive solution for potential users.

The OAPF identifies an additional location within the Isle of Dogs and South Poplar Opportunity Area for a potential freight consolidation initiative. It is noted that there is currently a live application being considered by Tower Hamlets Council at this location - Orchard Wharf, Orchard Place, Poplar, London, E14 0JY (PA/20/02488). The application proposes the following:

*"Part A - Full planning application for redevelopment of site following demolition of all existing buildings and enabling works to provide a mixed-use development consisting of the erection of five buildings between 15 and 30 storeys (56.6 m AOD and 103.75 m AOD) above a raised safeguarded wharf box (15.5m AOD) and one standalone 20 storey building (68.9 m AOD) which would deliver: (i) a total of up to 826 dwellings (Class C3) and ancillary accommodation; (ii) up to 8,212 m2 gross internal area (GIA) of General Industrial / Storage or Distribution floorspace (Class B2/B8) including ancillary office accommodation; and (iii) 135 m2 (GIA) of flexible commercial floorspace (Class E).*

*Associated works include hard and soft landscaping; private amenity space; vehicular access and servicing facilities; car parking and cycle parking; and other works incidental to the proposals including works to the River Wall;*

*And Part B - Outline planning application for external waterborne freight infrastructure and all other related works (including marine works) for which all matters are reserved."*

Included in the application is the proposal to deliver a last mile urban logistics facility that would enable goods and products to be transported to the site on the River Thames and then distributed onwards to Central London and beyond. The proposed use would constitute a waterborne freight infrastructure initiative utilising the River Thames as a transport network in the movement of goods and products. Any future freight and logistics study will need to consider the determination of this live application with the potential for both sites to work together as part of a wider network.

The successful integration of an urban freight network across the Opportunity Area would contribute to providing a co-ordinated approach to delivery and servicing, reducing vehicle movements, de-congesting roads and improving air quality.

### YOU SAID...

We need to provide space within South Poplar for local entrepreneurs.



### WHAT WE DID...

We are encouraging developments to make provision for a variety of workspace types and sizes, this will support local creatives, makers and doers as well as small to medium sized enterprises.

## ACTIVE FRONTAGES

Active frontages allow active visual engagement between the street and ground/upper floors of a building. Active uses can include (but are not limited to) retail shops, cafes and other social uses. Development which does not provide active uses in accordance with this policy must demonstrate how the proposed use would ensure appropriate activation of the area and integration between the street and building. Active frontages can be created on a range of uses which can be broadly categorised as non-residential and residential. Active frontages should be provided with regard to the indicative layout depicted in Figure 69.

Active uses can include (but are not limited to) retail shops, cafes and other social uses. Development which does not provide active uses must demonstrate how the proposed use would ensure appropriate activation of the area and integration between the street and building.

Active frontages are essential to successful place-making, given their ability to create character and foster community cohesion by providing the opportunity for people to meet and engage in a social or economic sense.

### Active Frontages



Figure 65: Ground floor retail spills out onto a tidy street section including a dedicated bike lane, parking, and trees, with residential above. Overschie, Rotterdam

### Residential Active Frontages



Figure 67: Ground floor entrances and windows provide activity and passive surveillance. Defensive planting area creates a semi-private zone and adds privacy. St. Andrews, London

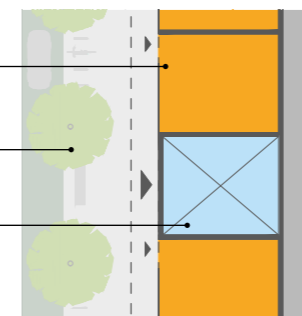
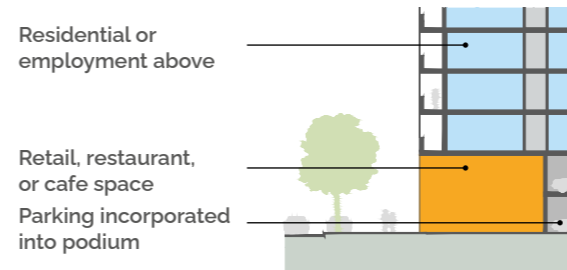


Figure 66: Indicative active frontage diagram

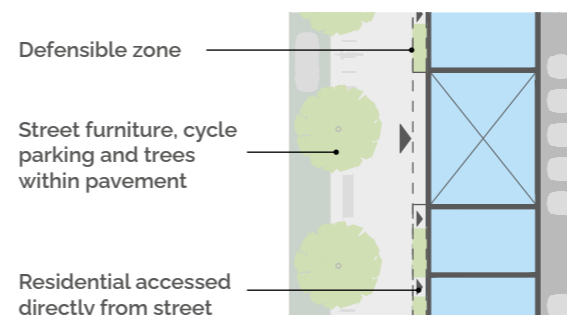


Figure 68: Indicative residential active frontage diagram



### DID YOU KNOW

The Tower Hamlets Local Plan and Planning Obligations SPD requires all major commercial and residential developments to secure jobs and apprenticeships for local residents during the construction and end-user phases of developments. There is also a requirement to secure a minimum of 20% of the total jobs created by the construction and end-user phases of new developments for local residents, and provide financial contributions for construction and end user phase skills training.

Landowners and developers are encouraged to develop coordinated approaches to skills training, jobs and apprenticeships; for example, providing shared construction skills training facilities to support multiple sites.



**Primary Active Frontages**

Designated Primary Active Frontages within the Masterplan area should be provided along key routes and public spaces. These frontages should be activated through non-residential uses including comparison retail, convenience shopping\*, community, café, restaurant or leisure uses as shown in the figure opposite. Blank facades and service access should be avoided along these frontages.

**Secondary Active Frontages**

Non-residential active uses should be promoted within the designated Secondary Active Frontages. The impact of residential entrances on non-residential frontages should be minimized and private areas and entrances should be clearly distinguishable.

Frontages used for the provision of servicing, bin, storage and plant access should be minimal. However if proposed, it must be demonstrated that such features would not detract from the overall quality of the building or public realm.

**Residential Active Frontages**

Residential Active Frontages should be provided across predominantly residential areas where there would be reduced commercial activity. Development along residential streets and routes should provide frontages with passive surveillance through the placement of entrances and orientation of windows. Areas of defensible space should be incorporated to provide a degree of privacy where individual residential units have entrances from the public realm.

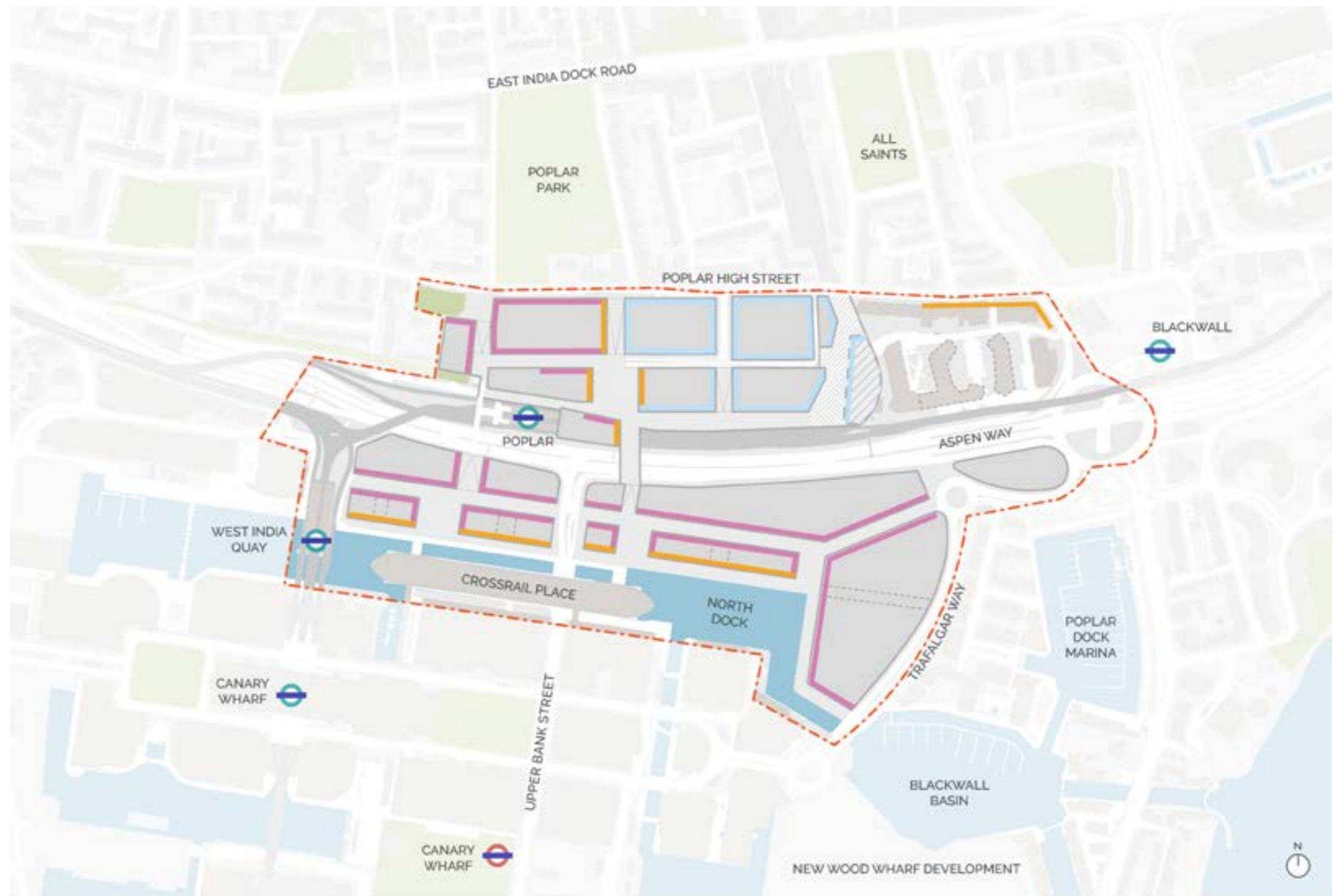


Figure 6g: Illustrative active frontages strategy

- - - - MASTERPLAN SITE AREA
- ▬ PRIMARY ACTIVE FRONTAGE
- ▬ SECONDARY ACTIVE FRONTAGE
- ▬ RESIDENTIAL ACTIVE FRONTAGE
- INDICATIVE URBAN BLOCK
- - - - POTENTIAL BREAK IN BUILDING LINE
- POTENTIAL ADDITIONAL OVERBUILD AREA
- POTENTIAL ADDITIONAL URBAN BLOCK

## 4.6 LIVEABILITY AND SUSTAINABILITY – SP6

The following local plan policies informed SP6 of the South Poplar Masterplan:

### Local Plan Policy Links

- Policy D.DH2 Attractive streets, spaces and public realm
- Policy D.TC5 Food, drink, entertainment and the night-time economy
- Policy S.CF1 Supporting community facilities
- Policy S.OWS1 Creating a network of open spaces
- Policy D.OWS4 Water spaces
- Policy S.ES1 Protecting and enhancing our environment
- Policy D.ES2 Air quality
- Policy D.ES3 Urban greening and biodiversity
- Policy D.ES4 Flood risk
- Policy D.ES5 Sustainable drainage
- Policy D.ES6 Sustainable water and wastewater management
- Policy D.ES7 A zero carbon borough
- Policy S.TR1 Sustainable travel

### SP6

In accordance with Tower Hamlets Local Plan policies, development should contribute to the delivery of sustainable design outcomes and liveable neighbourhoods. To achieve this, the following supplementary guidance for development within the South Poplar Masterplan in relation to Liveability and Sustainability is provided:

1. Development should respond to the following liveability and sustainability principles:
  - a. Pedestrian priority + Scale
  - b. 24/7/365 Activity
  - c. Active Streets
  - d. Views and daylight
  - e. Good-Microclimate
  - f. Quality Homes
  - g. Water management

### YOU SAID...

Safety across the neighbourhood should be improved through good design, especially around Poplar DLR Station, where anti-social behaviour is an issue (Online Exhibition).



### WHAT WE DID...

Development is expected to increase the activity on streets and routes, including around Poplar DLR station and to add 'eyes on the street' to increase the safety of the public realm.

The term sustainability is used in many different contexts. This guidance provides an approach to creating liveable and sustainable neighbourhoods in terms of the human dimension in urban life. The plan opposite illustrates how this guidance can be applied to promote a liveable high, density neighbourhood for South Poplar.

People's priorities are shifting. Health and wellbeing, sustainability, and work-life balance are becoming increasingly important. More people are choosing to live near their work or choosing active modes of transport. However, often there are physical and social barriers that means not everyone has access to the same choices.

The Council has ambitious targets to deliver reductions in carbon emissions in keeping with the goal to become a zero-carbon borough. Developments will be expected to adhere to requirements in reducing carbon dioxide emissions on site and through off-setting measures in accordance with Policy D.ES7 of the Local Plan. Development should also deliver on the Council's biodiversity goals in accordance with Policy D.ES3 of the Local Plan and the Tower Hamlets Local Biodiversity Action Plan 2019-2024

The masterplan promotes neighbourhoods where everyone has access to essential services and amenities within a 15-minute walking distance. Prioritising high-density, mixed use buildings contribute to compact neighbourhoods. This allows more people to live and work near transport hubs and more local services supported by increased population density.

High-density does not always mean tall buildings, as medium-rise developments are more likely to deliver better amenity for residents. They provide a human scale and can create a better sense of community and intimacy.

Within the masterplan area there are appropriate places for tall buildings. Outside of the Tall Building Zone, these should be incorporated into urban blocks that positively define streets, animate public

spaces with active uses and provide amenities including quality open spaces for residents.

The most up-to-date best-practice principles around placemaking, sustainability and liveability should be incorporated into development schemes at the time of application submission.

### Water management

The growth expected to come forward across the Isle of Dogs and South Poplar poses significant challenges on the existing and planned water services across the area. With existing water services infrastructure nearing or already at capacity, impacts on water quality and flooding risk is a key concern. The Isle of Dogs and South Poplar Integrated Water Management Plan (IWMP) was published in October 2020 by the Greater London Authority in partnership with the London Borough of Tower Hamlets. The document provides a series of recommendations and measures that can be implemented across residential and commercial development schemes to sustainably manage water supply, demand and drainage.

The published IWMP contains an Appendix E (Developer Checklist) which provides developers with a summary of how water management should be considered in the planning and design of development proposals. The water management policies have been set out in the South Poplar Masterplan in accordance with the IWMP and provide additional guidance to policies adopted in the Council's Local Plan, in particular policies S.ES1, S.ES3, D.ES5, D.ES6, S.OWS1 and S.OWS3.



## 1. PEDESTRIAN PRIORITY + SCALE

- Intuitive way-finding and clearly communicated pedestrian priority in public space
- The extension of the urban network should be apparent and people should know they are walking to a high quality urban environment
- Deprioritise cars by avoiding roundabouts, turning lanes, rounded corners
- Take a balanced view of the needs of all potential user groups
- Support a walkable, human scale with increased permeability

## 2. 24/7/365 ACTIVITY

- Canary Wharf Major Centre and adjacent Activity Area should have a mixed programme and avoid 'dead areas' in the evening
- Support Canary Wharf's redesignation as a Metropolitan Centre with a diverse mix of leisure and employment uses
- Consolidate retail frontages to increase their impact

## 3. ACTIVE STREETS

- Support continuous streetscapes with back-of-pavement buildings
- All blocks should have a variety of openings and points of interaction
- Blocks along key routes and public spaces should have multiple uses and entrances at ground level
- Residential streets should have entrances directly off the street and windows overlooking the public realm
- Blank and service areas along facades should be minimised

## 4. VIEWS AND DAYLIGHT

- Maximise dual-aspect homes and avoid single aspect north-facing homes
- Create breaks in residential blocks and offset the position of taller elements
- Promote shallow-plan office design which meets BREEAM daylight criteria

## 5. GOOD-MICROCLIMATE

- Support public open spaces in locations with good sun conditions
- Distribute volume and massing so that all streets and public spaces have adequate direct sunlight exposure, for example by creating a consistent shoulder height and setting taller elements back to improve the quality of streets

## 6. QUALITY HOMES

- Integrate a number of different residential typologies including apartments, studios, townhomes and maisonettes
- Provide a variety of private and semi-private amenity spaces such as balconies, terraces and communal courtyards

## 7. WATER MANAGEMENT

- Contribute to the provision of sustainable drainage systems, increase water resilience and protect and enhance the water environment. This can be achieved by:
  - Exploring the installation and use of rainwater harvesting, grey water re-use and dual plumbing for future external non-potable water supply;
  - Exploring the potential for innovative water re-use between building occupiers and adjacent plots;

### YOU SAID...

New tall buildings should not impact the sunlight in South Poplar's public spaces.



### WHAT WE DID...

Included a requirement that new developments maintain the liveability of the area, including respecting existing daylight and sunlight.

- Exploring gravity fed options for water re-use for high volume users at lower development levels, such as the train care depot.
- Contribute to reducing flood risk and improving water quality by reducing discharge volumes to the combined sewer system. This can be achieved by:
  - Exploring innovative solutions with New Appointment Variation (NAV) providers to remove surface and grey water discharge to the sewer system and promote its re-use;
  - Collaborating with adjacent landowners to explore the discharge of surface water run-off to the North Dock or the River Thames in co-ordination with the Canal and River Trust and/or the Environment Agency and landowners en-route;
  - Ensure surface water runoff is drained to a tanked attenuation system and control systems to allow water reuse are installed; and
  - Explore the installation of dual plumbing

New development along Poplar High Street should be aligned with back-of-pavement to create a consistent streetscape

Potential for additional future north-south connections to increase permeability

Offset the position of taller elements to minimise overlooking, improve views and the environmental quality of private amenity space

Create a new continuous north-south pedestrian route from Crossrail to Poplar High Street with a new bridge over Aspen Way

Improve legibility and accessibility of the route from Poplar High Street to Poplar Station through landscaped stairs and ramps

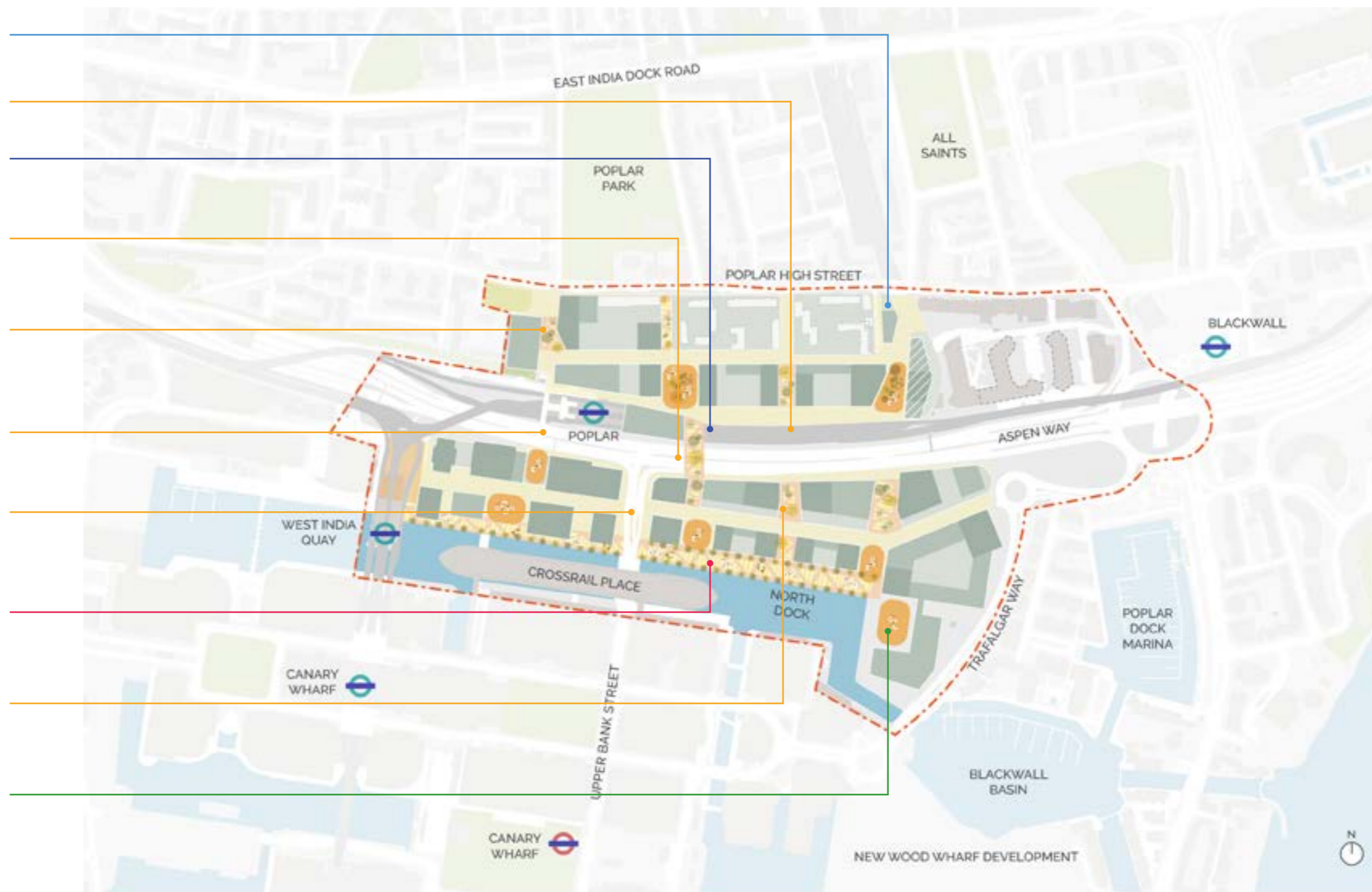
Upgrade or replace the existing Aspen Way Footbridge from Poplar Station to North Quay and create generous arrival spaces

Create a pedestrian connection from Upper Bank Street to North Quay and Billingsgate sites and to the dockside promenade. Provide clear crossing with good pedestrian conditions.

Provide a generous and continuous dockside promenade activated by retail, cafes, restaurants and leisure uses

Aspiration for potential future overbuild of Aspen Way to integrate development north-south, improve connectivity and environment for surrounding developments

Situate public open space in locations with good sun conditions, for example capitalising on the opportunity for a new west-facing public space on axis with North Dock



- MASTERPLAN SITE AREA
- KEY PUBLIC OPEN SPACE
- TALLER ELEMENT
- PLINTH WITH CONSISTENT SHOULDER HEIGHT
- PODIUM / LOWER ELEMENTS
- PUBLIC REALM
- POTENTIAL ADDITIONAL BLOCK
- EXISTING RETAINED BUILDINGS
- PERMITTED DEVELOPMENT



# 5 DELIVERY AND INFRASTRUCTURE PLAN

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DELIVERY, FUNDING AND MONITORING  
INFRASTRUCTURE PHASING AND PRIORITIES  
IMPLEMENTATION OVER TIME

## 5.1 DELIVERY

The vision set out within the South Poplar Masterplan contains a number of key physical and social infrastructure initiatives. The provision of such infrastructure is a critical component of sustainable masterplanning and placemaking. There are also additional studies and initiatives recommended within this masterplan that can assist the further regeneration of the area.

The infrastructure requirements, studies and recommendations contained within this masterplan are supported by policies and strategies within existing and draft planning policy documents at a national, regional and local level.

To support the delivery of infrastructure across the masterplan area through the development management process and to ensure key opportunities are unlocked, an Infrastructure Phasing and Priority Plan (page 55) has been prepared. This plan provides the description of individual projects, rationale, strategic priority, approximate timescales, potential delivery bodies/partners and likely funding streams. Development applications within the South Poplar Masterplan area will need to respond to this plan as well as the requirements within the Local Plan.

The area covered by the South Poplar Masterplan has multiple landowners, creating challenges in the practical delivery of physical and social interventions recommended within the masterplan. It is therefore incumbent on landowners and authorities to work together through a collaborative approach in the design and development of schemes, supporting physical and social

infrastructure, public realm, open space and other services. The Council is committed to working productively with landowners, developers and key partners through the development management process to ensure a co-ordinated approach.

The South Poplar Masterplan will primarily be delivered by the following Delivery Bodies:

### London Borough of Tower Hamlets (LBTH)

The Tower Hamlets Council is the local planning authority with jurisdiction over the South Poplar Masterplan area. The Council has important statutory planning and regulatory powers that can enable major and comprehensive regeneration opportunities to be delivered.

### Key Landowners

The cooperation of landowners and organisations will be critical in ensuring the vision of the Masterplan and key regeneration opportunities are delivered. Key landowners within the masterplan area include the London Borough of Tower Hamlets, City of London Corporation, Canary Wharf Group, Transport for London (Property Development), New City College, East End Community Foundation, UK Power Networks and Housing Associations.

### Other Key Partners

Transport for London, the Greater London Authority, Housing Associations, the Canal and River Trust, Thames Water, Local Businesses, Community and Faith Organisations and Charities and South Poplar Residents.

## 5.2 FUNDING

To fund the delivery of the principles and strategies within the Masterplan, local, regional and central government authorities will need to work collaboratively with landowners and developers. The following funding and resource opportunities will need to be utilised to secure the provision of strategic infrastructure including:

### Development Industry

Landowners and developers for sites within the masterplan area will provide the primary funding of infrastructure through the delivery of development schemes. The appropriate provision for and funding of infrastructure opportunities will be determined through the development management process along with other funding sources.

### Community Infrastructure Levy (CIL)

This Levy is a charge applied to most types of new developments over a certain size. These payments can assist in funding local infrastructure projects that are needed to support new developments, such as schools, connections, health services, leisure, open spaces, transport improvements and further studies.

### Section 106 Agreements

Section 106 Agreements otherwise known as 'planning obligations' are legally binding agreements that can be made in connection with planning permissions. Agreements contain obligations, either financial or non-financial, where it is necessary to offset or mitigate the impacts caused by development. It would be possible to secure financial or non-financial obligations to support the provision of infrastructure across the masterplan area.

### Other Public Sector

Other public sector funding streams could arise, such as through the Housing Infrastructure Fund, other Central Government schemes, Homes England, GLA, TfL or through local and regional authority schemes. These public sector funding bodies may have the ability to assist in bringing forward new homes, community facilities and infrastructure.

The infrastructure requirements for the Local Plan Site Allocations of the North Quay and Billingsgate Market sites have been assessed to be sufficiently viable to deliver as per the London Borough of Tower Hamlets Local Plan Viability Assessment (December 2017 – BNP Paribas Real Estate).



## 5.3 MONITORING

The Council monitors the effectiveness and suitability of policies in the Council's Annual Monitoring Report (AMR). The supplementary policies in the South Poplar Masterplan SPD will be monitored as part of this process, and where necessary will be updated or replaced.

## 5.4 INFRASTRUCTURE PHASING AND PRIORITY

The Infrastructure Phasing and Priority Plan has been informed by the Council's Infrastructure Delivery Plan (October 2017), the Local Implementation Plan (February 2019), the Isle of Dogs and South Poplar Development Infrastructure Funding Study (published October 2019), the Isle of Dogs and South Poplar OAPF Transport Strategy (published October 2019) and the Isle of Dogs and South Poplar Integrated Water Management Plan (October 2020). The plan will inform the development of the South Poplar Masterplan area which will consider local, strategic and regional infrastructure needs.

As a significant amount of growth is expected to come forward across the South Poplar Masterplan SPD Area, it is important to ensure this growth is supported by sufficient social and physical infrastructure.

Given the local, strategic and regional importance of infrastructure in this area, the Council acknowledges that there is a need for significant investment. The Infrastructure Phasing and Priority Plan provides a summary of infrastructure projects and initiatives that will require investment in the short, medium and long term to support existing and new communities within the area. It is anticipated that this plan will aid in the negotiation of planning applications through the development management process and promote investment

from funding sources at a local, regional and national level.

The policies in the National Planning Policy Framework (NPPF), the London Plan, and Tower Hamlets Local Plan set out to ensure that the infrastructure requirements of new development are delivered as part of the development management process.

The Tower Hamlets Infrastructure Delivery Plan is updated annually and contains additionally detailed information on specific projects.

**Phasing Period:** Phasing refers to when relevant infrastructure or further work is expected to come forward within the short term (up to 2024); medium term (up to 2028); and long term (post 2028 and 2031).

**Priority:** Priority refers to how critical it is to deliver the infrastructure or additional work element.

1. Critical enabling
2. Essential mitigation
3. High priority
4. Desirable

## 5.5 INFRASTRUCTURE PHASING AND PRIORITY PLAN

Key	Project Description	Project Rationale	Delivery Bodies + Partners	Potential Funding Streams	Priority	Approximate Timescale and Phasing
1	Upgrade Poplar footbridge and associated connections south towards the Canary Wharf Elizabeth Line Station and to the north towards Poplar High Street.	Upgraded, improved and refurbished Poplar Footbridge to be developed as adjacent development sites come forward.  This would improve connectivity for new and existing residents by linking Poplar High Street, through the footbridge and to the Canary Wharf Elizabeth Line Station.	CWG, NCC, EECF, TfL, LBTH	CIL/S106, Other	1	Short term.  As development comes forward.
2	East/West dockside connection route	Continuous, landscaped east-west connection along the dockside animated and activated by retail, cafes and restaurants. This route will improve wide pedestrian connectivity. This has the potential to link through to Trafalgar Way and to the Wood Wharf development.	Development sites, LBTH	CIL/S106, Other	1	Short to medium term.
3	Land bridge over Aspen Way (1 bridge) with associated connections south towards the Canary Wharf Elizabeth Line Station and to the north towards Poplar High Street.	To support the delivery of a north – south link between South Poplar and Canary Wharf.	Central Government, LBTH, TfL, Development sites	Central Government (Housing Infrastructure Fund), CIL, S106, TfL, Other	1	Medium term.  As development comes forward.
4	Poplar Station East	Additional station entrance and works to Poplar Station, to be delivered as part of the future development of Poplar DLR depot.	Development Sites, TfL, LBTH	TfL, Other	2	Long term.
5	East/West cycling connection along Aspen Way	This would provide an additional cycling connection through the borough and provide an alternative to the Cycle Super Highway 3 located along Poplar High Street.	Development sites, LBTH, TfL	CIL, S106, Other	1	Short to medium term.
6	Additional land bridges across Aspen Way (2nd and 3rd bridge)	To support the further delivery of a north – south links between South Poplar and Canary Wharf.	Development sites, TfL, LBTH	CIL, S106, TfL, Other	4	Long term.
7	Deck structure over Aspen Way	A deck over structure would provide significant public benefit to the masterplan area and wider regional context.	Development sites, TfL, LBTH	Further investigation required	4	Long term.
8	East/west central access route (South of Aspen Way)	This connection would provide thoroughfare access and link the sites of North Quay and the Billingsgate Market and then through to Trafalgar Way and beyond  Vehicular servicing access may also be possible.	Development sites, LBTH	CIL, S106, Other	1	Short to medium term.
9	Public Open Space	To meet additional requirements of the provision of green parks and open space. This would support mixed, sustainable and healthy communities. In particular, meeting the Aspen Way, North Quay and Billingsgate Market provision of parks and open space as per Local Plan Site Allocations.	Development sites, LBTH	CIL, S106, Other	1	Short to medium term.  As development sites come forward.

Table 1: Infrastructure phasing and priority



Key	Project Description	Project Rationale	Delivery Bodies + Partners	Potential Funding Streams	Priority	Approximate Timescale and Phasing
10	DLR Station Public Realm upgrades	Improvements to the public realm areas of the Poplar DLR Station.	LBTH, TfL, Development sites	TfL, Other	2	Medium Term.
11	Cycle Hire	Increased cycle hire stations in new locations at North Quay, Billingsgate and on the potential decking over Aspen Way. Cycle Hire stations would come forward as part of the planning process (if assessed to be appropriate and subject to land availability), as a response to increased demand as a result of additional development.	Development sites, LBTH, TfL	CIL, S106, Other	3	As development comes forward.
12	Connections to CS3	Upgraded cycling facilities on routes connecting to CS3. Such as upgrades between CS3 and A1020 and Poplar High Street. Rerouting Ming Street section to Penny Fields	Development sites, TfL, LBTH	CIL, S106, TfL, Other	2	Medium term.
13	Secondary School/ Education Provision	In accordance with the site allocation requirement for the Billingsgate Market, a secondary school or educational provision to be provided to meet borough-wide need.  Education demand and requirement to be determined at the time of application submission.	LBTH, Development sites	CIL, S106, Other	3	Medium to long term.  As the development site comes forward.
14	Integrated Water Management	Measures and recommendations as provided within the Isle of dogs and South Poplar Integrated Water Management Plan.	Canal and River Trust, Thames Water, NAVs	CIL, S106, Other	1	Short to long term.
15	Poplar High Street	Place making study for Poplar High Street (including junction improvements). This will investigate enhancements to the role of Poplar High Street within the local area and reviewing its longer term movement function.	LBTH, TfL	CIL, Other	1	Medium to long term
16	Cotton Street/Poplar High Street & Blackwall Connections	Improved facilities of pedestrians, cyclists and public realm enhancements along Cotton Street and Preston's Road. Improved wider connectivity to Blackwall Station.	LBTH, TfL, development sites	CIL, Other	1	Short to medium term.
17	Creative/cultural strategy and wayfinding	A strategy for creative and cultural regeneration to inform the design of the public realm, streets and spaces and create places of exception and delight across the Isle of Dogs Opportunity area.	Development sites, LBTH, TfL	CIL, S106, Other	2	Medium to long term.
18	Greening and Biodiversity Strategy	Strategy for greening throughout the masterplan including provision of 'living building' elements, retaining and enhancing habitats and features of biodiversity value, native tree planting, soft landscaping, SUDS and rain gardens. This will improve the private and public areas across the area whilst also improving the biodiversity value of the borough.	Development sites, LBTH	CIL, S106, Other	1	Short term.  As development sites come forward.
19	Freight Strategy	Develop freight and logistics strategy and evidence base, building on the Masterplan and OAPF to inform freight management decision making.  Incept a freight coordination forum across the Isle of Dogs and South Poplar Opportunity Area to consider the evidence base and to coordinate freight activities.	LBTH, TfL, GLA, Development Sites	CIL, S106, Other	1	Short to long term.
20	Prestons Road Roundabout	Redesign of roundabout to provide improvements to pedestrian access and connectivity. Public realm, walking and cycling improvements surrounding the roundabout and underpass.	LBTH, TfL, Development sites	CIL, S106, Other	2	Medium to long term.

### Priority 1

- ① Upgrade Poplar footbridge and connections
- ② East/west dockside connection route
- ③ Land bridge over Aspen Way (1 bridge)
- ⑤ Aspen Way east/west cycling route
- ⑧ North Quay Way east/west route
- ⑨ Public Open Space (site wide)
- ⑩ DLR Station Public Realm upgrades
- ⑭ Integrated Water Management (site wide)
- ⑮ Poplar High Street
- ⑯ Cotton Street/Poplar High Street & Blackwall
- ⑰ Greening and Biodiversity Strategy (site wide)
- ⑱ Freight Strategy (site wide)

### Priority 2

- ④ Poplar Station East
- ⑫ Connections to CS3
- ⑰ Cultural strategy and wayfinding (site wide)

### Priority 3

- ⑪ Cycle Hire
- ⑬ Secondary School/Education Provision
- ⑳ Prestons Road Roundabout

### Priority 4

- ⑥ Additional land bridges across Aspen Way
- ⑦ Deck structure over Aspen Way



Figure 70: Illustrative diagram showing location of key infrastructure

- INDICATIVE URBAN BLOCK
- POTENTIAL BREAK IN BUILDING LINE
- ▨ POTENTIAL ADDITIONAL OVERBUILD AREA
- ▩ POTENTIAL ADDITIONAL URBAN BLOCK



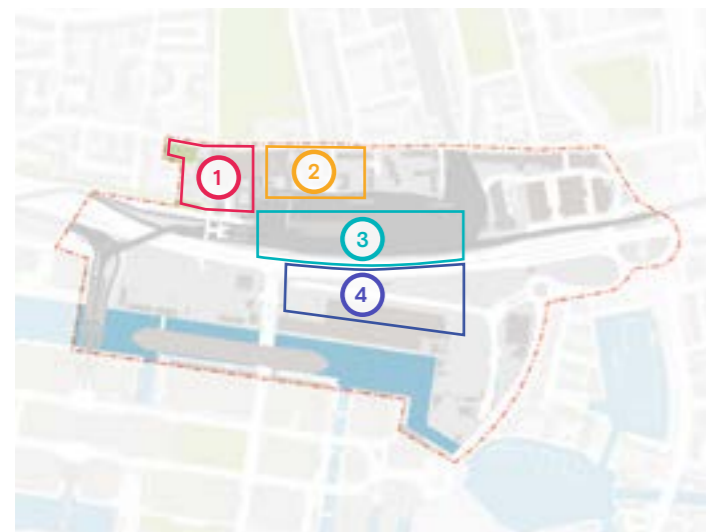
## 5.6 IMPLEMENTATION OVER TIME

It is challenging to discuss 'phasing' across such a large, complex site with multiple landowners and stakeholders. Many sites may come forward simultaneously or development may come forward piece by piece across several decades.

As development sites come forward, it is crucial a joined-up, coordinated approach is taken that enables the wider vision for the masterplan area to be delivered.

The following sets out possible approaches and consideration to key areas within the site that will unlock further potential and provide a cohesive piece of urbanism as shown in Figure 33.

These key areas are shown below and the preferred approach, challenges and alternatives set out opposite.



1. Upgraded route from Poplar High Street to Poplar Station with access to depot overbuild
2. Second north-south connection from Poplar High Street
3. Depot overbuild and potential future Aspen Way overbuild
4. Development of Billingsgate to enable potential future overbuild of Aspen Way

### 1. Implementation considerations for an upgraded Poplar Station route

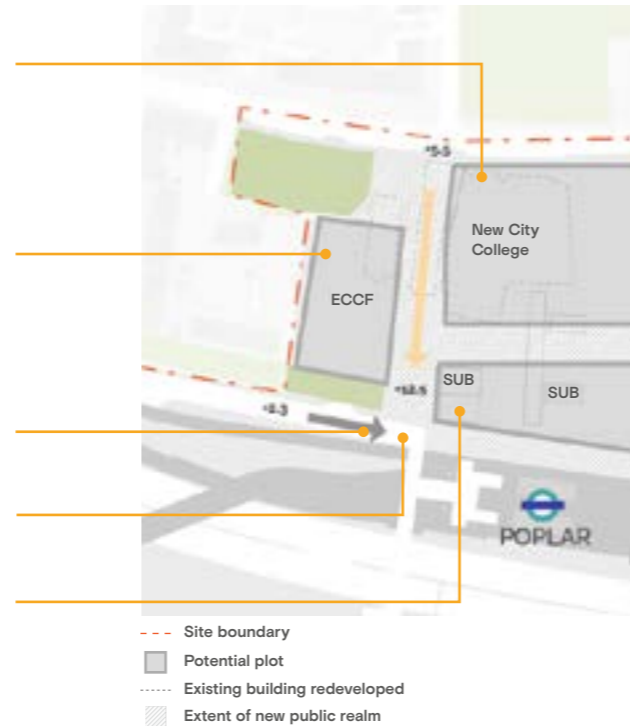
Wider route to accommodate a combination of steps and ramps to create level access requires reconfiguration of western edge of New City College building

Potential to upgrade and enhance EECF facilities in a new, purpose built building which 'bookends' new route. This could take place as part of a potential mixed-used redevelopment of the site

Maintain existing access to DLR depot from Castor Lane

Design of route to accommodate a potential future wider footbridge

Overbuild/incorporation of substations subject to access and safety considerations



### 2. Options and implementation consideration for a secondary connection from Poplar High Street

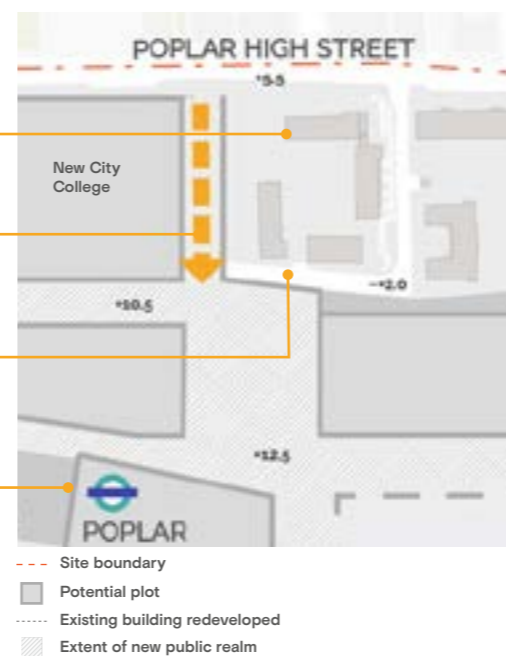
#### Option A

Retention of all existing housing estates along Poplar High Street

New connection from Poplar High Street to DLR overbuild and potential new station entrance

Retained existing access route from Simpson's Road

Allow for provision of secondary, eastern entrance to Poplar Station



### 3\*4. Enabling future overbuild of DLR depot and Aspen Way



Position building line on TfL depot site to allow future overbuild development

Potential to encapsulate rising DLR viaduct into overbuild development subject to operational and safety considerations

Provide ground floor spaces and uses suitable for future overbuild, such as local freight solutions and servicing

Potential DLR and Aspen Way overbuild development

#### Option B

Create a new landscaped pocket park in line with Woodstock Terrace. Level of intervention dependent on location and width of new route. Option shown with reconfiguration of Vietnamese Pastoral Centre

Continuation of east-west route gradually rising to a new central square

Allow for provision of secondary, eastern entrance to Poplar Station



